THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Eric S. Brown Digitally signed by Eric S. Brown
Date: 2024.01.09 15:58:32 -06'00'

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TENNESSEE DEPARTMENT OF TRANSPORTATION
BUILDING A, FIRST FLOOR
300 BENCHMARK PLACE
JACKSON, TN 38301
ERIC S. BROWN, P.E. NO. 120440

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.


| SHEET NAME | SHEET NO. |
|---|---|
| SIGNATURE SHEET | ROADWAY-SIGN1 |
| TITLE SHEET | 1 |
| ROADWAY INDEX AND STANDARD DRAWINGS | 1A |
| ESTIMATED ROADWAY QUANTITIES | 2 |
| TYPICAL SECTIONS AND PAVEMENT SCHEDULE | 2B |
| GENERAL NOTES | 2C |
| SPECIAL NOTES | 2D |
| ENVIRONMENTAL NOTES | 2E, 2E1 |
| TABULATED QUANTITIES | 2F |
| UTILITY NOTES AND UTILITY OWNERS | 3 |
| PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL | 4 |


| YEAR | PROJECT NO. | SHEET NO. |
|---|---|---|
| 2024 | 66S022-S8-004 | ROADWAY-SIGN1 |

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

08-JAN-2024 13:07
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Index Of Sheets
SEE SHEET NO. 1A

# STATE OF TENNESSEE
# DEPARTMENT OF TRANSPORTATION
# BUREAU OF ENGINEERING

| DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86 | YES | NO X |
|---|---|---|
| WORK ZONE SIGNIFICANCE DETERMINATION | | |
| SIGNIFICANT | YES | NO X |

| TENN. | YEAR | SHEET NO. |
|---|---|---|
| | 2024 | 1 |
| STATE PROJ. NO. | 66S022-S8-004 | |
| STATE PROJ. NO. | 66S022-M3-006 | |

ADJACENT PROJECT
PIN 131917.00 L.M. 0.00 TO L.M. 5.47 RESURFACING

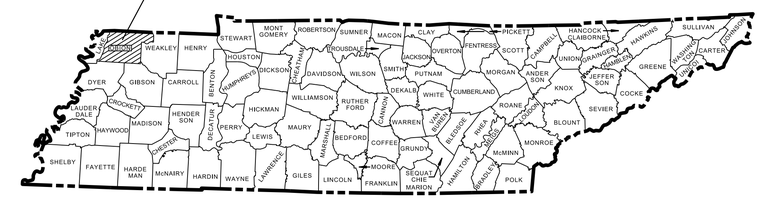
# OBION COUNTY

SR-22
FROM: L.M. 5.47 (SR-5)
TO: L.M. 26.80 (SR-21)

## RESURFACE, SAFETY AND BRIDGE REPAIR
COLD PLANE, PAVE, PAVEMENT MARKINGS, GUARDRAIL

STATE HIGHWAY NO. 22 F.A.H.S. NO. N/A

PROJECT LOCATION



BRIDGE ID. # 66SR0220007 @ L.M. 7.76 (HOOSIER CREEK)
66SR0220009 @ L.M. 15.63 (NORTH REELFOOT CREEK)
66SR0220011 @ L.M. 16.12 (BRANCH)
66SR0220013 @ L.M. 17.98 (BRANCH)
66SR0220015 @ L.M. 18.60 (REELFOOT CREEK)
66SR0220017 @ L.M. 18.79 (OVERFLOW)
66SR0220019 @ L.M. 21.59 (BRANCH)
66SR0220021 @ L.M. 23.35 (INDIAN CREEK)
66SR0220023 @ L.M. 26.73 (BRANCH)

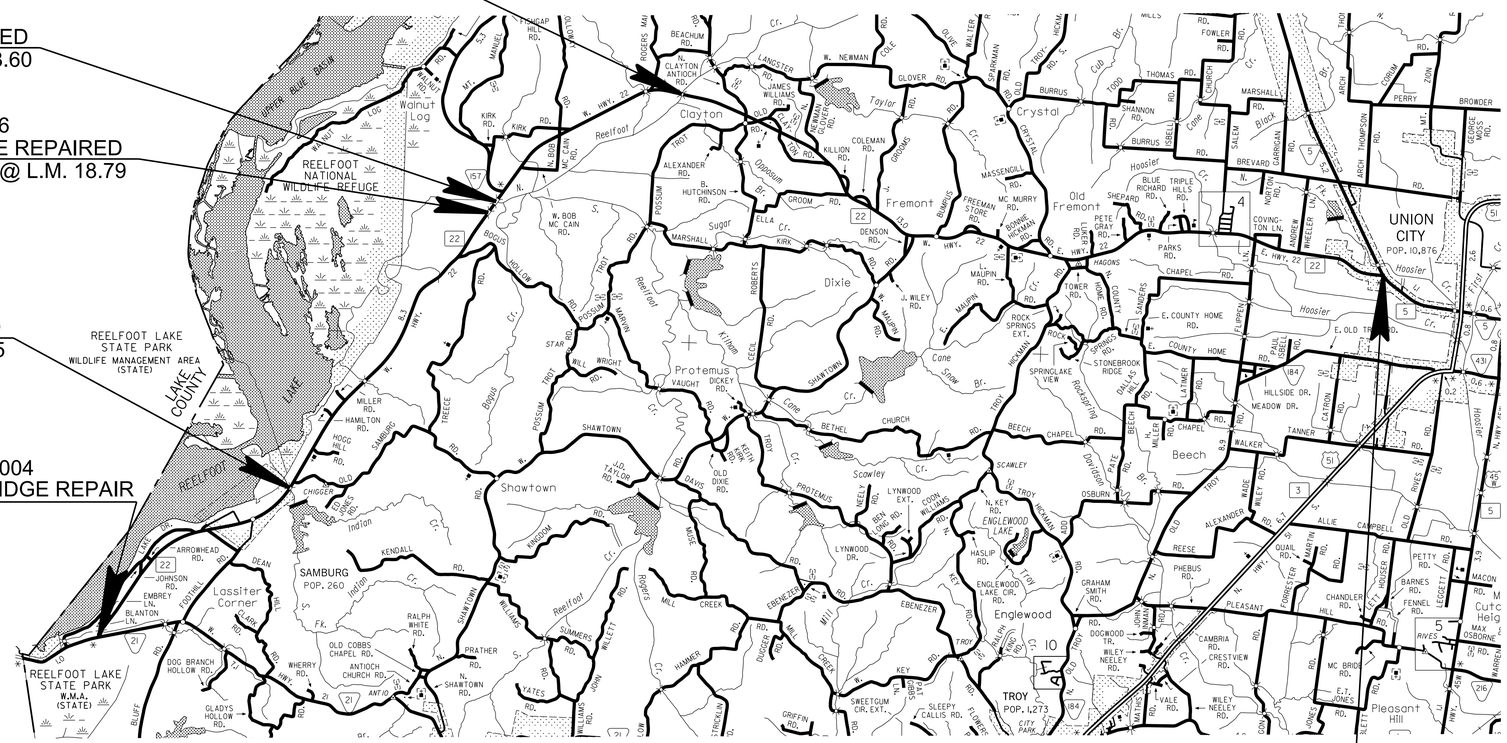
66S022-M3-006
BRIDGE TO BE REPAIRED
66SR0220009 @ L.M. 15.63
(NORTH REELFOOT CREEK)

66S022-M3-006
BRIDGE TO BE REPAIRED
66SR0220015 @ L.M. 18.60
(REELFOOT CREEK)

66S022-M3-006
BRIDGE TO BE REPAIRED
66SR0220017 @ L.M. 18.79
(OVERFLOW)

66S022-M3-006
BRIDGE TO BE REPAIRED
66SR0220021 @ L.M. 23.35
(INDIAN CREEK)

END PROJECT NO. 66S022-S8-004
RESURFACE, SAFETY AND BRIDGE REPAIR
L.M. 26.80 (SR-21)




BEGIN PROJECT NO. 66S022-S8-004 RESURFACE, SAFETY AND BRIDGE REPAIR
L.M. 5.47 (SR-5)

NO EXCLUSIONS

SEALED BY
ERIC S. BROWN REGISTERED ENGINEER AGRICULTURE COMMERCE NO. 120640 STATE OF TENNESSEE
1/09/2024

APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, COMMISSIONER

## SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT ROAD SP. SV. 2 : ERIC S. BROWN, P.E., REG. 4

DESIGNER : GINA GOLIGHTLY, REG. 4 CHECKED BY : NICHOLAS STEPHENS, P.E., REG. 4

P.E. NO. 98043-4283-04

PIN NO. 133812.00

PROJECT LENGTH 21.33 MILES
TOTAL LANE MILES RESURFACED 42.04 MILES

| TRAFFIC DATA | |
|---|---|
| ADT (2023) | 3533 |
| POSTED SPEED LIMITS | |
| L.M. 5.47 - L.M. 6.12 | 45 MPH |
| L.M. 6.12 - L.M. 23.36 | 55 MPH |
| L.M. 23.36 - L.M. 24.00 | 45 MPH |
| L.M. 24.00 - L.M. 24.63 | 35 MPH |
| L.M. 24.63 - L.M. 26.80 | 55 MPH |

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED:

DIVISION ADMINISTRATOR DATE

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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|---|---|---|---|
| RESURF. | 2024 | 66S022-S8-004 | 1A |
| | | | |
| | | | |
| | | | |

# ROADWAY INDEX


| SHEET NAME | SHEET NO. |
|---|---|
| SIGNATURE SHEETS | ROADWAY-SIGN1 |
| TITLE SHEET | 1 |
| ROADWAY INDEX AND STANDARD DRAWINGS | 1A |
| ESTIMATED ROADWAY QUANTITIES | 2 |
| TYPICAL SECTIONS AND PAVEMENT SCHEDULE | 2B |
| GENERAL NOTES | 2C |
| SPECIAL NOTES | 2D |
| ENVIRONMENTAL NOTES | 2E, 2E1 |
| TABULATED QUANTITIES | 2F |
| UTILITY NOTES AND UTILITY OWNERS | 3 |
| PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL | 4 |
| ACCESS MANAGEMENT DETAILS | 5-7 |
| BRIDGE REPAIR PLANS | B1 |


NO UTILITY ADJUSTMENTS IN THIS PLAN SET.

NO PROJECT COMMITMENTS IN THIS PLAN SET.

# STANDARD ROADWAY DRAWINGS

| DWG. | REV. | DESCRIPTION |
|---|---|---|
| **ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS** | | |
| RD-TP-1 | 09-26-16 | STANDARD ROADWAY DRAWINGS TITLE SHEET |
| RD-A-1 | 02-20-20 | STANDARD ABBREVIATIONS A THROUGH L |
| RD-A-2 | | STANDARD ABBREVIATIONS M THROUGH Z |
| RD-L-1 | 02-20-20 | STANDARD LEGEND |
| RD-L-1A | | STANDARD LEGEND |
| **SAFETY DESIGN AND GUARDRAILS** | | |
| S-GR-31-1 | 06-15-21 | GUARDRAIL DETAILS |
| S-GR-31-1A | 06-28-19 | GUARDRAIL AND BLOCK-OUT DETAILS |
| S-GR-31-1B | | GUARDRAIL FASTENING HARDWARE |
| S-GR-31-1C | 07-07-23 | GUARDRAIL GENERAL NOTES AND POST DETAILS |
| S-GRA-4 | 03-01-23 | IN-LINE GUARDRAIL ANCHOR |
| S-GRS-4 | 05-04-22 | SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL |
| S-GRT-2 | 06-28-19 | TYPE 38 GUARDRAIL END TERMINAL |
| S-GRT-2R | 06-28-19 | EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINALS (RETROFIT) |
| **DESIGN - TRAFFIC CONTROL** | | |
| T-M-1 | 06-28-19 | DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS |
| T-M-2 | 06-28-19 | DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS |
| T-M-3 | 07-07-23 | MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS |
| T-M-4 | 07-17-20 | STANDARD INTERSECTION PAVEMENT MARKINGS |
| T-M-15A | 06-28-19 | ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED RURAL ROUTES |
| T-M-16 | 03-04-21 | RUMBLE STRIPE INSTALLATION LAYOUT |
| T-M-16A | 02-03-20 | RUMBLE STRIPE DETAILS FOR EDGE OF PAVEMENT AND CENTERLINE |
| T-WZ-10 | 04-02-12 | ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS |

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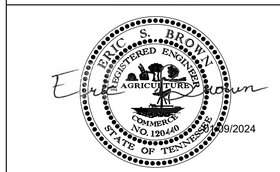


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
DRAWINGS

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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|---|---|---|---|
| RESURF. | 2024 | 66S022-S8-004 | 2 |
| | | | |
| | | | |
| | | | |

## ESTIMATED ROADWAY QUANTITIES

| | ITEM NO. | DESCRIPTION | UNIT | QUANTITY 66S022-S8-004 |
|---|---|---|---|---|
| | 202-03.01 | REMOVAL OF ASPHALT PAVEMENT | S.Y. | 1100 |
| | 203-06 | WATER | M.G. | 47 |
| | 208-01.05 | BROOMING & DEGRASSING SHOULDERS | L.M. | 41.995 |
| (1) | 303-02 | MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D) | TON | 6309 |
| (2) | 307-02.01 | ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING A | TON | 506 |
| (3) | 307-02.08 | ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING B-M2 | TON | 35343 |
| | 403-02.01 | TRACKLESS TACK COAT | TON | 279 |
| (4) | 411-01.21 | LONGITUDINAL JOINT SEALANT | L.M. | 22.879 |
| (5) | 411-02.10 | ACS MIX(PG70-22) GRADING D | TON | 23254 |
| (6) | 411-12.01 | SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH) | L.M. | 0.83 |
| | 411-12.04 | SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (4IN WIDTH) | L.M. | 29.551 |
| (7) | 415-01.01 | COLD PLANING BITUMINOUS PAVEMENT | TON | 54538 |
| (8) | 705-02.10 | GUARDRAIL TRANSITION 27IN TO 31IN | EACH | 25 |
| (8) | 705-04.09 | EARTH PAD FOR TYPE 38 GR END TREATMENT | EACH | 1 |
| (8) | 705-06.20 | TANGENT ENERGY ABSORBING TERM MASH TL-3 | EACH | 25 |
| (8) | 706-01 | GUARDRAIL REMOVED | L.F. | 1250 |
| (8) | 706-06.03 | RADIUS RAIL | L.F. | 25 |
| (8) | 706-10.26 | ROUNDED END ELEMENT | EACH | 1 |
| | 712-01 | TRAFFIC CONTROL | LS | 1 |
| (8) | 712-06 | SIGNS (CONSTRUCTION) | S.F. | 5302 |
| | 713-16.01 | CHANGEABLE MESSAGE SIGN UNIT | EACH | 2 |
| | 716-01.21 | SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR) | EACH | 1414 |
| | 716-01.30 | REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER | EACH | 1414 |
| (9)(10)(11) | 716-02.04 | PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING) | S.Y. | 178 |
| (9)(10) | 716-02.05 | PLASTIC PAVEMENT MARKING (STOP LINE) | L.F. | 659 |
| (9)(10) | 716-02.07 | PLASTIC PAVEMENT MARKING (24" BARRIER LINE) | L.F. | 113 |
| (9)(10) | 716-03.03 | PLASTIC WORD PAVEMENT MARKING (STOP AHEAD) | EACH | 2 |
| (9)(10)(12) | 716-04.04 | PLASTIC PAVEMENT MARKING (TRANSVERSE SHOULDER) | L.F. | 565 |
| (9)(10) | 716-04.12 | PLASTIC PAVEMENT MARKING (YIELD LINE) | S.F. | 27 |
| (13) | 716-05.04 | PAINTED PAVEMENT MARKING (CHANNELIZATION STRIPING) | S.Y. | 70 |
| (13) | 716-05.05 | PAINTED PAVEMENT MARKING (STOP LINE) | L.F. | 58 |
| (13) | 716-05.07 | PAINTED PAVEMENT MARKING (24" BARRIER LINE) | L.F. | 113 |
| (13) | 716-05.20 | PAINTED PAVEMENT MARKING (6" LINE) | L.M. | 144.112 |
| (9) | 716-12.02 | ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE) | L.M. | 72.056 |
| | 717-01 | MOBILIZATION | LS | 1 |
| | | | | |

NOTE: NO UTILITY ADJUSTMENTS ON THIS PROJECT.

## FOOTNOTES

(1) USE AS DIRECTED BY THE TDOT ENGINEER, INCLUDES 744 TONS FOR BREAKOUT.

(2) FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01.

(3) INCLUDES 1511 TONS FOR EXTRA WIDTH PAVEMENT, FROM L.M. 13.72 TO L.M. 26.80, INCLUDING SIDE ROADS, INTERSECTIONS, PRIVATE DRIVES, FIELD ENTRANCES AND BUSINESS ENTRANCES, AND 13 TONS FOR DEEP MILLING.

(4) TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE TDOT ENGINEER. USE CRAFTCO PAVEMENT JOINT ADHESIVE #34524, PAVON JOINT ADHESIVE BY PAVON CORPORATION, OR DENSO TAPE BY DENSO.

(5) INCLUDES 2076 TONS FOR EXTRA WIDTH PAVEMENT FOR SIDE ROADS, INTERSECTIONS, PRIVATE DRIVES, FIELD ENTRANCES AND BUSINESS ENTRANCES, AND 1350 TONS FOR SPOT LEVELING.

(6) TO BE USED AT L.M. 18.48 TO L.M. 19.03.

(7) INCLUDES 3461 TONS FOR EXTRA WIDTH PAVEMENT FOR SIDE ROADS, INTERSECTIONS, PRIVATE DRIVES, FIELD ENTRANCES AND BUSINESS ENTRANCES, AND 12 TONS FOR DEEP MILLING.

(8) SEE TABULATED QUANTITIES, SHEET 2F, FOR DETAILS.

(9) FOR FINAL PAVEMENT MARKING ONLY.

(10) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.

(11) INCLUDES 107 SY FOR ACCESS MANAGEMENT PAVEMENT MARKINGS, SEE SHEETS 5-7, FOR DETAILS.

(12) TO BE USED FOR ACCESS MANAGEMENT PAVEMENT MARKINGS, SEE SHEETS 5-7, FOR DETAILS.

(13) FOR USE AS TEMPORARY LINE MARKINGS.

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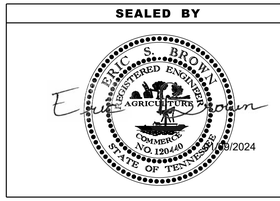


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED ROADWAY QUANTITIES

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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|---|---|---|---|
| RESURF. | 2024 | 66S022-S8-004 | 2B |
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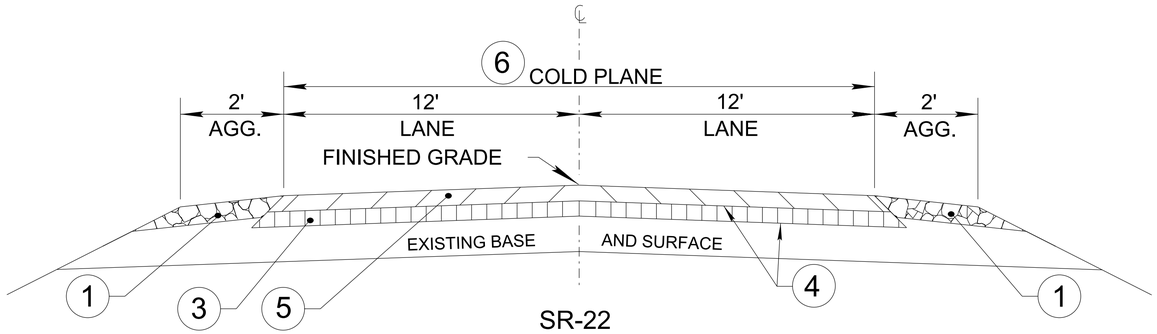
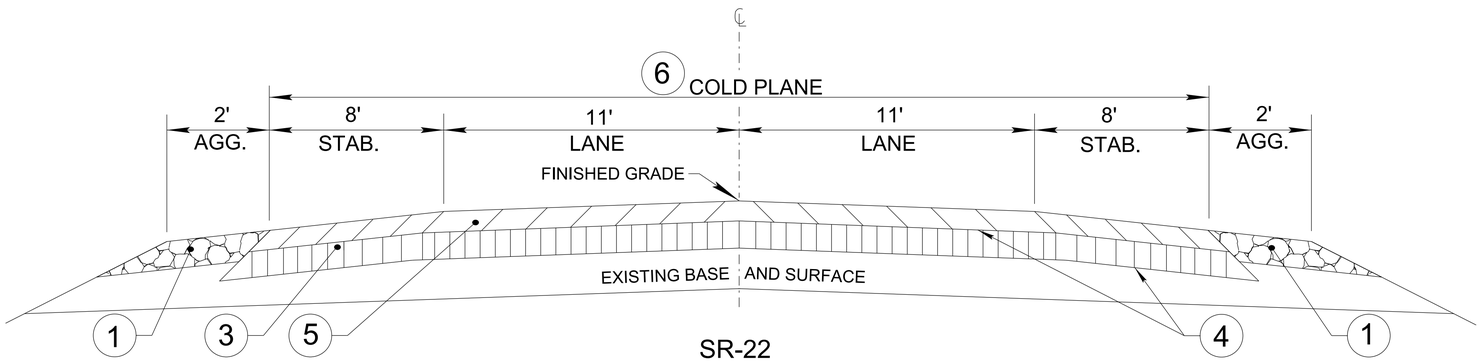
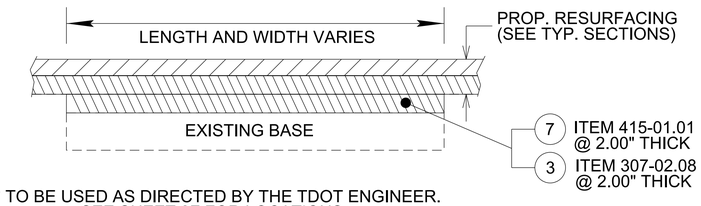



SR-22
FROM L.M. 5.47 TO L.M. 18.48
FROM L.M. 19.03 TO L.M. 26.80




SR-22
FROM L.M. 18.48 TO L.M. 19.03

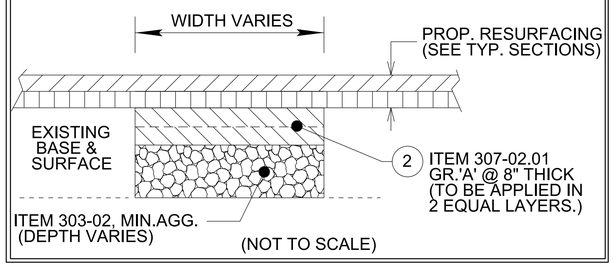
## DEEP MILLING / ASPHALT REPAIR DETAIL




** TO BE USED AS DIRECTED BY THE TDOT ENGINEER.
SEE SHEET 2F FOR LOCATIONS.

(NOT TO SCALE)

## DETAIL OF BREAKOUT

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## PROPOSED PAVEMENT SCHEDULE

1. MINERAL AGGREGATE BASE @ 2.00"± THICK FOR SHOULDERS
   ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING (C OR D)
2. BITUMINOUS PLANT MIX BASE (HOT MIX) PG70-22 GRADING "A" @ 8.00"± THICK (APPROX. 920.00 LBS./S.Y.)
   ITEM 307-02.01 ASPHALT CONCRETE MIX (PG70-22)(BPMB-HM) GRADING "A"
   (THIS ITEM IS TO BE USED FOR BREAKOUT ONLY)
3. BITUMINOUS PLANT MIX BASE (HOT MIX) PG70-22 GRADING "B-M2" @ 2.00" THICK (APPROX. 226.00 LBS./S.Y.)
   ITEM 307-02.08 ASPHALT CONCRETE MIX (PG70-22) (BPMP-HM) GRADING B-M2
4. TACK COAT (TC)
   ITEM 403-02.01 TRACKLESS TACK COAT
   SEE TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 403.05
   FOR DETERMINING APPLICATION RATE IN THE FIELD
5. ASPHALTIC CONCRETE SURFACE (ACS) @ 1.25"± THICK (APPROX. 132.50 LBS./S.Y.)
   ITEM 411-02.10 ACS MIX(PG70-22) GRADING "D"
6. COLD PLANING @ 3.25"± THICK (APPROX. 341.25 LBS./S.Y.)
   ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
7. COLD PLANING @ 2.00"± THICK (APPROX. 210.00 LBS./S.Y.)
   ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT

## BRIDGE RECOMMENDATIONS

| LOCATION | BRIDGE NO. | LENGTH (FT.) | TREATMENT |
|---|---|---|---|
| LM 7.76 | 66SR0220007 | 85.50 | LEAVE AS IS |
| LM 15.63 | 66SR0220009 | 584.50 | SEE BRIDGE REPAIR PLANS FOR DETAILS |
| LM 16.12 | 66SR0220011 | 33.40 | COLD PLANE AND PAVE WITH PLANS MIX / TREATMENT TYPE |
| LM 17.98 | 66SR0220013 | 32.00 | COLD PLANE AND PAVE WITH PLANS MIX / TREATMENT TYPE |
| LM 18.60 | 66SR0220015 | 471.78 | SEE BRIDGE REPAIR PLANS FOR DETAILS |
| LM 18.79 | 66SR0220017 | 331.44 | SEE BRIDGE REPAIR PLANS FOR DETAILS |
| LM 21.59 | 66SR0220019 | 22.50 | PAVE WITH PLANS MIX / TREATMENT TYPE |
| LM 23.35 | 66SR0220021 | 142.50 | SEE BRIDGE REPAIR PLANS FOR DETAILS |
| LM 26.73 | 66SR0220023 | 51.00 | PAVE WITH PLANS MIX / TREATMENT TYPE |

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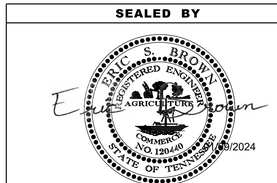


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|---|---|---|---|
| RESURF. | 2024 | 66S022-S8-004 | 2C |

# GENERAL NOTES

## GRADING

(1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

(3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

## GUARDRAIL

(1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

(3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

## MISCELLANEOUS

(2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.

(3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

## PAVEMENT MARKINGS

### TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

(2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

## FINAL PAVEMENT MARKING

(5) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED AND MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.

b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

## SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

(19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

## PAVEMENT

## PAVING

(2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

## RESURFACING

(4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.

(6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.

(9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

## SIGNING

(12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

## CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

(1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

(2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

(3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

(4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

(5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.

(6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.

(7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

(9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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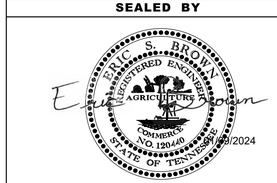


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GENERAL NOTES

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# SPECIAL NOTES

## PAVEMENT

### PAVING

(1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

(2) WHEN A PERFORMANCE GRADE (PG) ASPHALT MIX WITH PROPERTIES GREATER THAN THAT OF PG64-22 IS CALLED FOR ON RESURFACING PLANS AND IS THE ONLY ASPHALT GRADE ON THE PROJECT, THE CONTRACTOR HAS THE OPTION OF USING EITHER THE ASPHALT GRADE SHOWN IN THE PLANS OR AN ASPHALT GRADE EQUAL TO OR BETTER THAN PG64-22 FOR DRIVEWAYS AND BUSINESS ENTRANCES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE MATERIAL WILL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THE ASPHALT OR ASPHALT MIX. THE MATERIAL TICKETS SHALL BE MARKED "FOR DRIVEWAYS AND BUSINESS ENTRANCES ONLY" AT THE POINT OF MANUFACTURE.

(6) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

### PAVEMENT MARKING

(1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.

(10) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

### SIGNS

(1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

### MISCELLANEOUS

(1) ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

(3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

### JOINT SEALANTS

(1) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL.

(2) MANUFACTURER'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.

(3) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALIN. ALL JOINTS SHALL BE SWEPT OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.

(4) TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE DROM TRACKING.

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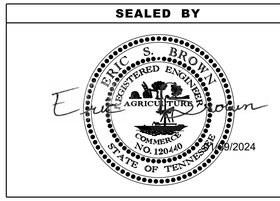


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SPECIAL NOTES

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# ENVIRONMENTAL NOTES

## SUBSECTION 1 – ENVIRONMENTAL GENERAL NOTES

## ENVIRONMENTAL GENERAL NOTES

### NATURAL RESOURCES

(4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.

(9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

### SPECIES

(11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).

(12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

### PERMITS, PLANS & RECORDS

(15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

## SUBSECTION 2 – ENVIRONMENTAL SPECIAL NOTES

## ENVIRONMENTAL SPECIAL NOTES

### ENVIRONMENTAL

(1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

### ECOLOGY

(2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.

(3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.

(4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

### SCOPE OF WORK

(6) THIS PROJECT INCLUDES COLD PLANING, PAVING, PAVEMENT MARKINGS, INSTALLATION OF JOINT SEALANT AND GUARDRAIL.

## SUBSECTION 3 – EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

## EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

### DISTURBED AREA

(1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

### SEDIMENT CONTROL

(6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

(8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

### GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

(29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.

(30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.

(31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.

(32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.

(33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.

(34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

(35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.

(36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.

(37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.

(38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.

(39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.

(40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

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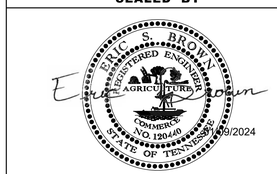


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# ENVIRONMENTAL NOTES

## SUBSECTION 3 – EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

## EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

### SPILL PREVENTION, MANAGEMENT & NOTIFICATION

(44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.

(45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.

(46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.

(47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.

(48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.

(49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.

(50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.

(51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

(52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

(53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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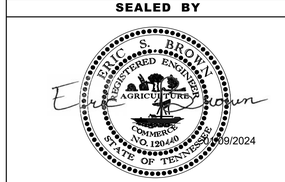


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL NOTES

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## GUARDRAIL TABULATION

| LOG MILE | SIDE | TRANSITION 705-02.10 (EACH) | EARTHPAD 705-04.09 (EACH) | MASH TL-3 705-06.20 (EACH) | GUARDRAIL REMOVED 706-01 (LF) | RADIUS RAIL 706-06.03 (LF) | ROUNDED END ELEMENT 706-10.26 (EACH) |
|---|---|---|---|---|---|---|---|
| 7.744 | L | 1 | | 1 | 50 | | |
| 7.788 | R | 1 | | 1 | 50 | | |
| 7.798 | L | 1 | | 1 | 50 | | |
| 9.859 | L | | | | | 25 | 1 |
| 10.308 | L | 1 | 1 | 1 | 50 | | |
| 10.463 | L | 1 | | 1 | 50 | | |
| 15.616 | L | 1 | | 1 | 50 | | |
| 15.616 | R | 1 | | 1 | 50 | | |
| 15.745 | L | 1 | | 1 | 50 | | |
| 15.747 | R | 1 | | 1 | 50 | | |
| 18.533 | R | 1 | | 1 | 50 | | |
| 18.578 | L | 1 | | 1 | 50 | | |
| 18.710 | R | 1 | | 1 | 50 | | |
| 18.773 | R | 1 | | 1 | 50 | | |
| 18.869 | L | 1 | | 1 | 50 | | |
| 18.869 | R | 1 | | 1 | 50 | | |
| 19.036 | L | 1 | | 1 | 50 | | |
| 19.036 | R | 1 | | 1 | 50 | | |
| 19.062 | L | 1 | | 1 | 50 | | |
| 21.578 | L AND R | 2 | | 2 | 100 | | |
| 21.611 | L AND R | 2 | | 2 | 100 | | |
| 23.336 | R | 1 | | 1 | 50 | | |
| 23.395 | L AND R | 2 | | 2 | 100 | | |
| **TOTAL** | | **25** | **1** | **25** | **1250** | **25** | **1** |

## SIGNS (CONSTRUCTION) 712-06

| QTY | MUTCD | DESCRIPTION | SIZE (IN X IN) | ITEM NO. 712-06 (S.F.) |
|---|---|---|---|---|
| 2 | G20-1 | ROAD WORK NEXT 22 MI. | 48 X 24 | 16 |
| 21 | G20-2 | END ROAD WORK | 48 X 24 | 168 |
| 18 | W8-1 | BUMP | 48 X 48 | 288 |
| 58 | W8-11 | UNEVEN LANES | 48 X 48 | 928 |
| 73 | W20-1 | ROAD WORK AHEAD | 48 X 48 | 1168 |
| 2 | W20-1 | ROAD WORK 1 MILE | 48 X 48 | 32 |
| 2 | W20-1 | ROAD WORK 1/2 MILE | 48 X 48 | 32 |
| 2 | W20-1 | ROAD WORK 1000 FT | 48 X 48 | 32 |
| 2 | W20-4 | ONE LANE ROAD AHEAD | 48 X 48 | 32 |
| 2 | W20-7 | FLAGGER | 48 X 48 | 32 |
| 2 | W20-7a | FLAGGER AHEAD | 48 X 48 | 32 |
| 2 | W21-2 | FRESH OIL | 48 X 48 | 32 |
| 44 | W21-5 | SHOULDER WORK | 48 X 48 | 704 |
| 86 | W8-15 | GROOVED PAVEMENT | 48 X 48 | 1376 |
| 86 | W8-15p | MOTORCYCLE PLAQUE | 30 X 24 | 430 |
| **TOTAL** | | | | **5302** |

THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.

## DEEP MILLING LOCATIONS

| L.M. | EB/WB | DIMENSIONS L x W |
|---|---|---|
| 21.0 | EB | 30' x 11' |
| 22.8 | WB | 30' x 11' |
| 26.6 | WB | 30' x 11' |

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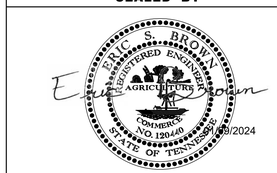


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED QUANTITIES

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# UTILITY NOTES

## UTILITY

(3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

# UTILITY OWNERS

**COMMUNICATIONS:**
**AT&T**
315 E COLLEGE ST
JACKSON, TN 38301
CONTACT: DANIEL R. POTTS
PHONE: 901-488-2359
EMAIL: db7607@att.com

**GAS:**
**LAKE COUNTY UTILITY DISTRICT**
123 N COURT ST
TIPTONVILLE, TN
CONTACT: JOEL BARGERY
PHONE: 731-253-6203
EMAIL: jbargerylcud@att.net

**ELECTRIC:**
**GIBSON COUNTY ELECTRIC**
1207 S COLLEGE ST
TRENTON, TN 38382
CONTACT: MIKE DAVIS
PHONE: 731-855-4740 EXT 1315
EMAIL: mdavis@gibsonemc.com

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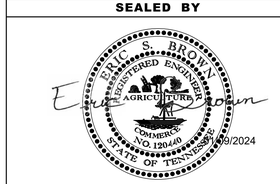


**STATE OF TENNESSEE**
**DEPARTMENT OF TRANSPORTATION**

UTILITY NOTES
AND
UTILITY OWNERS

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# PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

**A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:**

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

   a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

   b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

   c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

   d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

   a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

      (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

      (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

   b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

   c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

   IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

   a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

      (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

      (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

   IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

   b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

   c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

   d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

   FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

   SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

   IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

**B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:**

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

   a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

   a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

      (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

      (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

   a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

      (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

      (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

   b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

   THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

**C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:**

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

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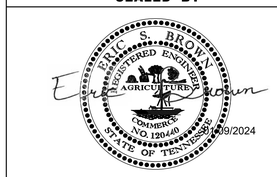


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

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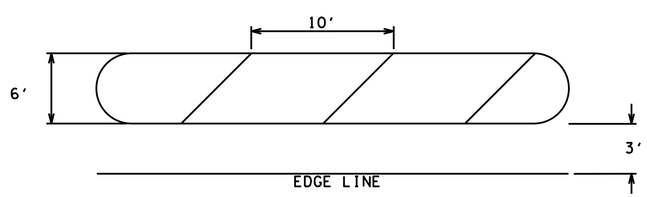


TYPICAL SECTION

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PAVEMENT MARKINGS DETAILED ON THIS SHEET SHALL BE WHITE

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DEPARTMENT OF TRANSPORTATION

ACCESS MANAGEMENT DETAILS

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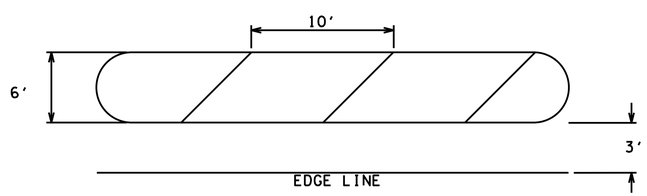


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ACCESS MANAGEMENT DETAILS

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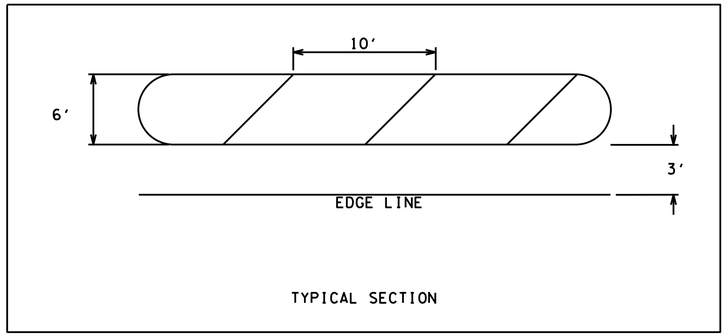


NOT TO SCALE

PAVEMENT MARKINGS DETAILED ON THIS SHEET SHALL BE WHITE

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ACCESS MANAGEMENT DETAILS

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BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
NO. 107196
STATE OF TENNESSEE

THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Brian Egli
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TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

| SHEET NAME | SHEET NO. |
|---|---|
| SIGNATURE SHEET | STRUCTURE-SIGN 3 |
| BRIDGE PLANS | B1 THRU B14 |

| YEAR | PROJECT NO. | SHEET NO. |
|---|---|---|
| 2024 | 66S022-M3-006 | STRUCTURE-SIGN 3 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 11/14/23 | ZH | ADDED SHEET |

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

$$$$SYTIME$$$$$$
$$$$DGNSPEC$$$$$$

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NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

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| SHEET NAME | SHEET NO. |
|---|---|
| SIGNATURE SHEET | STRUCTURE-SIGN 2 |
| BRIDGE PLANS | B1 THRU B5 |

| YEAR | PROJECT NO. | SHEET NO. |
|---|---|---|
| 2024 | 66S022-M3-006 | STRUCTURE-SIGN 2 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 10/11/23 | ZH | ADDED SHEET |

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

$$$$SYTIME$$$$$
$$$DGNSPEC$$$

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
NO. 107196
STATE OF TENNESSEE

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TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

| SHEET NAME | SHEET NO. |
|---|---|
| SIGNATURE SHEET | STRUCTURE-SIGN 1 |
| BRIDGE PLANS | B1 THRU B5 |

| YEAR | PROJECT NO. | SHEET NO. |
|---|---|---|
| 2023 | 66S022-M3-006 | STRUCTURE-SIGN 1 |

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

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| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B1 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 10/11/23 | ZH | ADDED REVISION DATES & REVISED TITLE & YEAR |
| 2 | 11/14/23 | ZH | ADDED B3-B11, REV. DATES & REF. DRAWINGS, REVISED TITLES |

## INDEX OF DRAWINGS


| | DWG. NO. | LAST REV. DATE |
|---|---|---|
| INDEX OF DRAWINGS | B1 | 11-14-2023 |
| BRIDGE TABULATION, ESTIMATED QUANTITIES, AND EXPANSION JOINT REPAIR NOTES | B2 | 11-14-2023 |
| EXPANSION JOINT DETAILS AT APPROACH PAVEMENT ENDS | B3 | 11-14-2023 |
| PLAN VIEW (66SR0220009) | B4 | 11-14-2023 |
| PHASE CONSTRUCTION (SPANS 1,3,4,6,7,12,13,14,& 15) | B5 | 11-14-2023 |
| PHASE CONSTRUCTION (SPANS 2,5,8,& 18) | B6 | 11-14-2023 |
| PHASE CONSTRUCTION (SPANS 9-11,16-17, & 19-20) | B7 | 11-14-2023 |
| PLAN VIEW (66SR0220015) | B8 | 11-14-2023 |
| PHASE CONSTRUCTION | B9 | 11-14-2023 |
| PLAN VIEW (66SR0220017) | B10 | 11-14-2023 |
| PHASE CONSTRUCTION | B11 | 11-14-2023 |
| PLAN VIEW (66SR0220021) | B12 | 11-14-2023 |
| PHASE CONSTRUCTION (SPANS 1, 2, 4, & 5) | B13 | 11-14-2023 |
| PHASE CONSTRUCTION (SPAN 3) | B14 | 11-14-2023 |


## INDEX OF REFERENCE DRAWINGS


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|---|---|
| LAYOUT OF BRIDGE | BR-88-05 |
| BRIDGE REPAIR DETAILS | BR-88-41 |
| BRIDGE REPAIR DETAILS | BR-88-43 |
| LAYOUT OF BRIDGE | BR-88-03 |
| LAYOUT OF BRIDGE | BR-88-04 |
| BRIDGE REPAIR DETAILS | BR-88-23 |
| BRIDGE REPAIR DETAILS | BR-88-25 |
| BRIDGE REPAIR DETAILS | BR-88-27 |
| LAYOUT | M-377-77 |
| SUPERSTRUCTURE | M-377-81 |
| LAYOUT | M-377-92 |
| SUPERSTRUCTURE | M-377-96 |


BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE & COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

INDEX OF DRAWINGS
66-SR22-15.63
OVER
OVER NORTH REELFOOT CREEK
66-SR22-18.60
OVER
REELFOOT CREEK
66-SR22-18.79
OVER
OVERFLOW
66-SR22-23.35
OVER
INDIAN CREEK
BR. NOS. 66SR0220009
66SR0220015
66SR0220017
66SR0220021
OBION COUNTY
2024

DESIGNED BY _____ DATE _____
DRAWN BY ZACH HAYNES DATE 6/23
SUPERVISED BY KEVIN MARTINKO DATE 6/23
CHECKED BY _____ DATE _____

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## TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES

| LOCATION OF BRIDGE AND BRIDGE NUMBER | REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS | TYPE OF WORK | 604-10.44 EXPANSION JOINT REPAIR L.F. |
|---|---|---|---|
| 66-SR22-15.63 OVER NORTH REELFOOT CREEK (66SR0220009) | BR-88-03 BR-88-04 BR-88-23 BR-88-25 BR-88-27 | EXPANSION JOINT REPAIR | 65 |
| 66-SR22-18.60 OVER REELFOOT CREEK (66SR0220015) | M-377-77 M-377-81 | EXPANSION JOINT REPAIR | 134 |
| 66-SR22-18.79 OVER OVERFLOW (66SR0220017) | M-377-92 M-377-96 | EXPANSION JOINT REPAIR | 134 |
| 66-SR22-23.35 OVER INDIAN CREEK (66SR0220021) | BR-88-05 BR-88-41 BR-88-43 | EXPANSION JOINT REPAIR | 65 |
| TOTAL | | | 398 |

PIN 133812.00

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B2 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 10/11/23 | ZH | REVISED TITLE & YEAR |
| 2 | 11/14/23 | ZH | ADDED BRIDGE TABS., QUANTITIES, NOTES & REVISED TITLES |

## EXPANSION JOINT REPAIR NOTES

(FOR 66SR0220009, 66SR0220015 @ APPROACH # 1, 66SR0220017 @ APPROACH # 1, AND 66SR0220021 @ BOTH ABUTMENTS)

THE JOINT HEADER SYSTEM SHALL BE FROM OPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM OPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM OPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM OPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE OPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF ¾ OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.

BRIAN K. EGLI REGISTERED ENGINEER AGRICULTURE COMMERCE No. 107196 STATE OF TENNESSEE

11/14/23

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION, ESTIMATED QUANTITIES, AND EXPANSION JOINT REPAIR NOTES
66-SR22-15.63 OVER OVER NORTH REELFOOT CREEK
66-SR22-18.60 OVER REELFOOT CREEK
66-SR22-18.79 OVER OVERFLOW
66-SR22-23.35 OVER INDIAN CREEK
BR. NOS. 66SR0220009
66SR0220015
66SR0220017
66SR0220021
OBION COUNTY
2024

B2

DESIGNED BY _____ DATE _____
DRAWN BY ZACH HAYNES DATE 6/23
SUPERVISED BY KEVIN MARTINKO DATE 6/23
CHECKED BY _____ DATE _____

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| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B3 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
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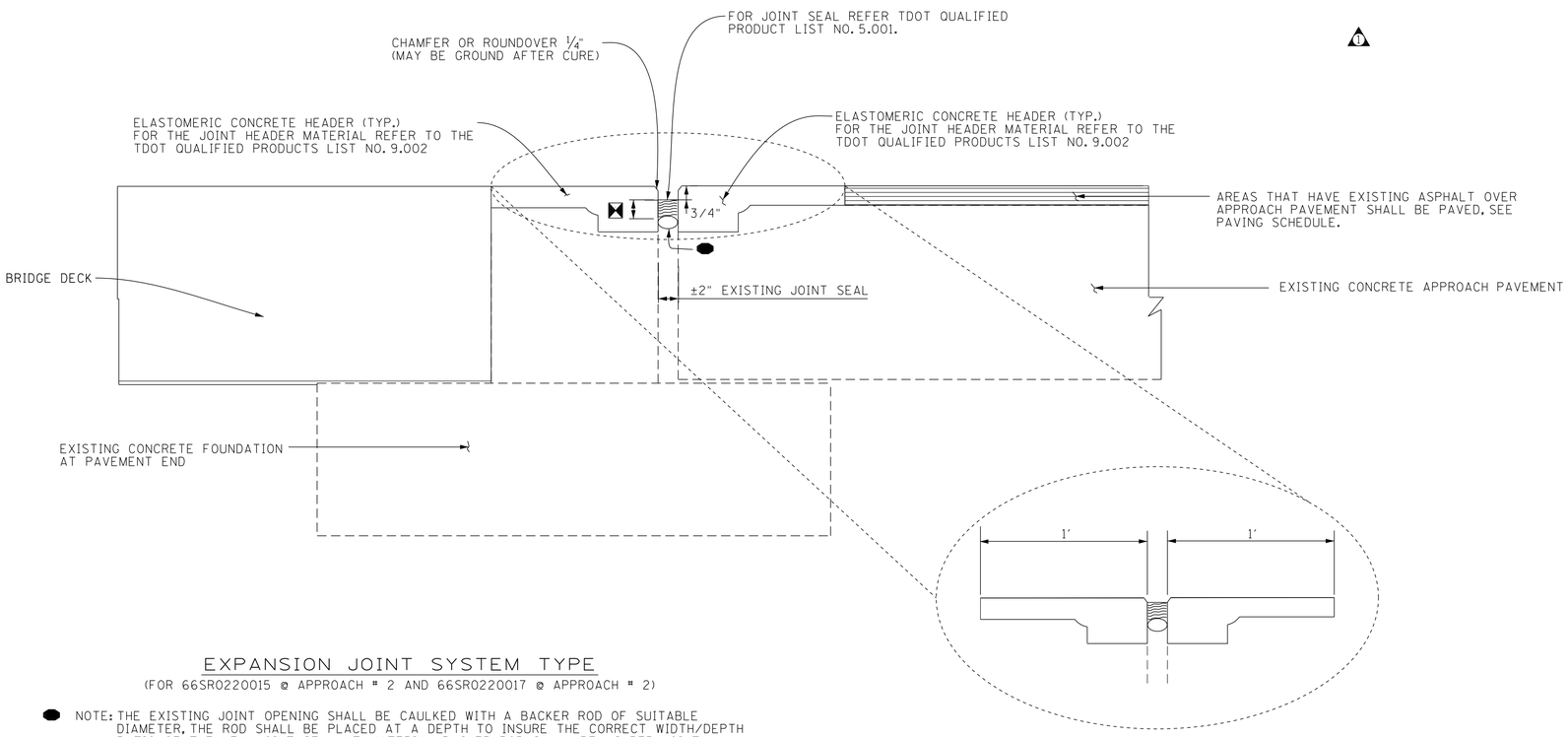


## EXPANSION JOINT SYSTEM TYPE

(FOR 66SR0220015 @ APPROACH # 2 AND 66SR0220017 @ APPROACH # 2)

NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER. THE ROD SHALL BE PLACED AT A DEPTH TO INSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL. BACKER ROD SHALL BE AS PER JOINT MANUFACTURER'S RECOMMENDATIONS.

NOTE: FULL DEPTH OF ALL EXISTING JOINTS SHALL SHALL BE RESEALED WITH NEW JOINT SEALER. THE NEW JOINT SEALER SHALL BE A COLD POUR TWO PART SILICONE SEALER FROM QPL 5.001.

THE JOINT HEADER SYSTEM SHALL BE FROM QPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM QPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM QPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM QPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE QPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

AFTER PAVING AND SEALING OVER THE EXISTING EXPANSION JOINT, THE ASPHALT SHALL BE CUT OUT AS SHOWN IN THE NEW EXPANSION JOINT DETAIL.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.

DESIGNED BY _____ DATE _____
DRAWN BY ZACH HAYNES DATE 11/23
SUPERVISED BY KEVIN MARTINKO DATE 11/23
CHECKED BY _____ DATE _____

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE & COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT
DETAILS AT APPROACH
PAVEMENT ENDS
66-SR22-15.63
OVER
OVER NORTH REELFOOT CREEK
66-SR22-18.60
OVER
REELFOOT CREEK
66-SR22-18.79
OVER
OVERFLOW
66-SR22-23.35
OVER
INDIAN CREEK
BR. NOS. 66SR0220009
66SR0220015
66SR0220017
66SR0220021
OBION COUNTY
2024



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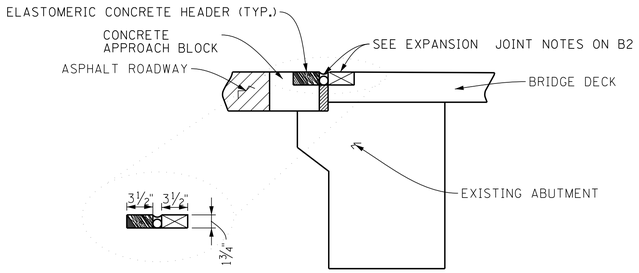
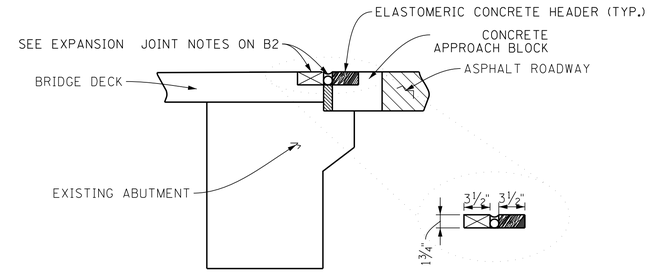



EXPANSION JOINT REPAIR DETAIL
@ ABUTMENT NO. 1




EXPANSION JOINT REPAIR DETAIL
@ ABUTMENT NO. 2

PIN 133812.00

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B4 |

REVISIONS

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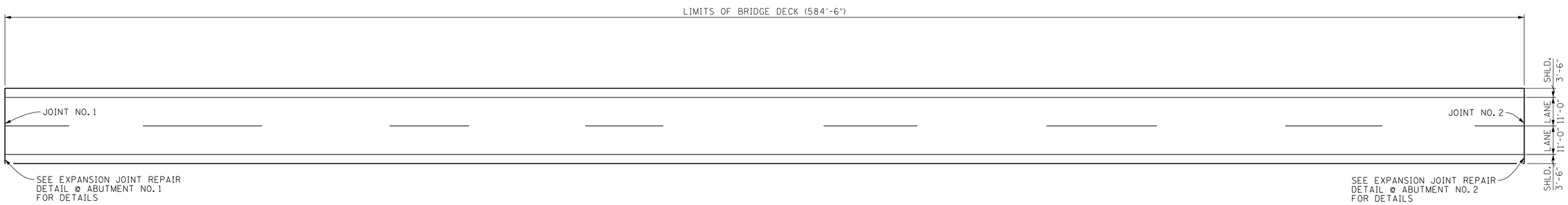


PLAN VIEW

BRIDGE ID. NO. 66SR0220009

11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
66-SR22-15.63
OVER
NORTH REELFOOT CREEK
BR. NO. 66SR0220009
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 11/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 11/23 |
| CHECKED BY | | DATE | |

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| PROJECT NO. | YEAR | SHEET NO. |
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| 66S022-M3-006 | 2024 | B5 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
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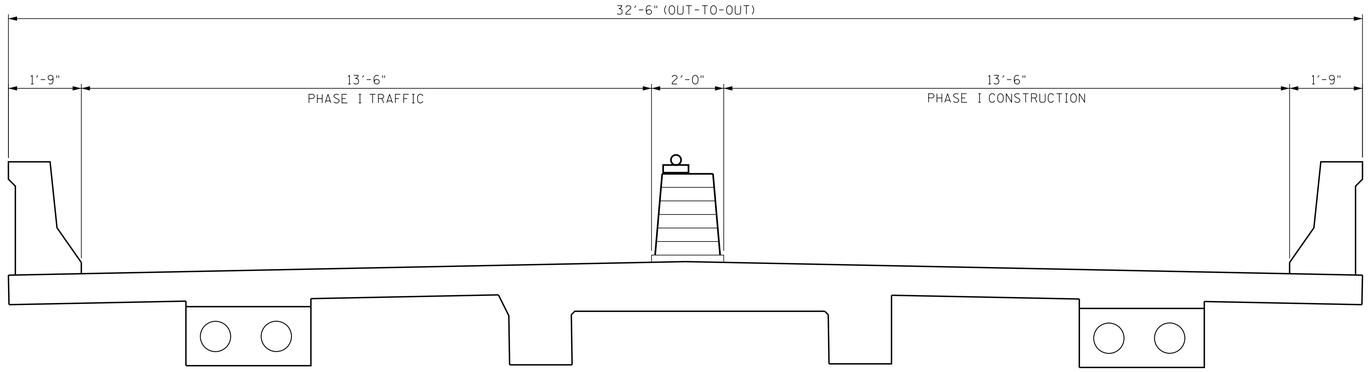
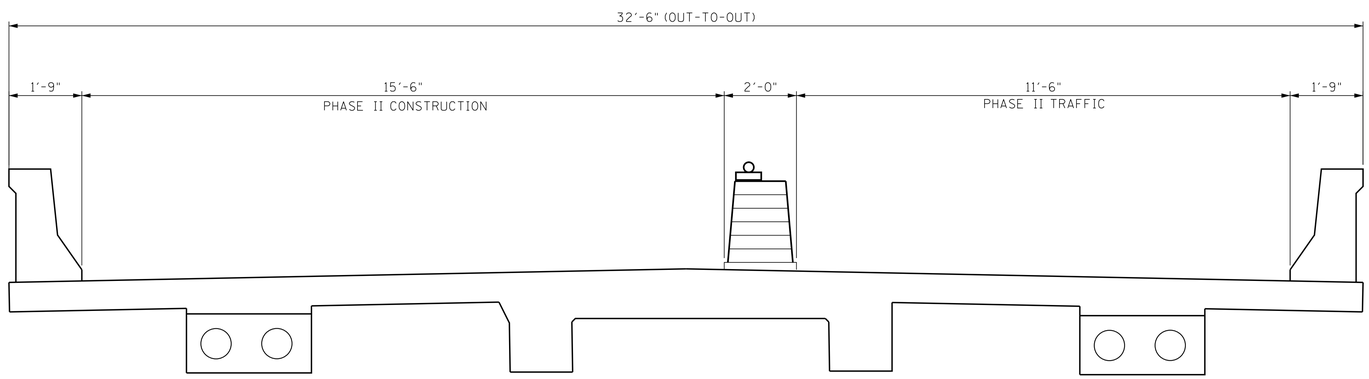



NORTHBOUND BRIDGE

(66-SR22-15.63)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)




NORTHBOUND BRIDGE

(66-SR22-15.63)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
SPANS 1,3,4,6,7,12,13,14,& 15
66-SR22-15.63
OVER
NORTH REELFOOT CREEK
BR. NO. 66SR0220009
OBION COUNTY
2024

| | | | |
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| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 11/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 11/23 |
| CHECKED BY | | DATE | |

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| PROJECT NO. | YEAR | SHEET NO. |
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| 66S022-M3-006 | 2024 | B6 |

REVISIONS

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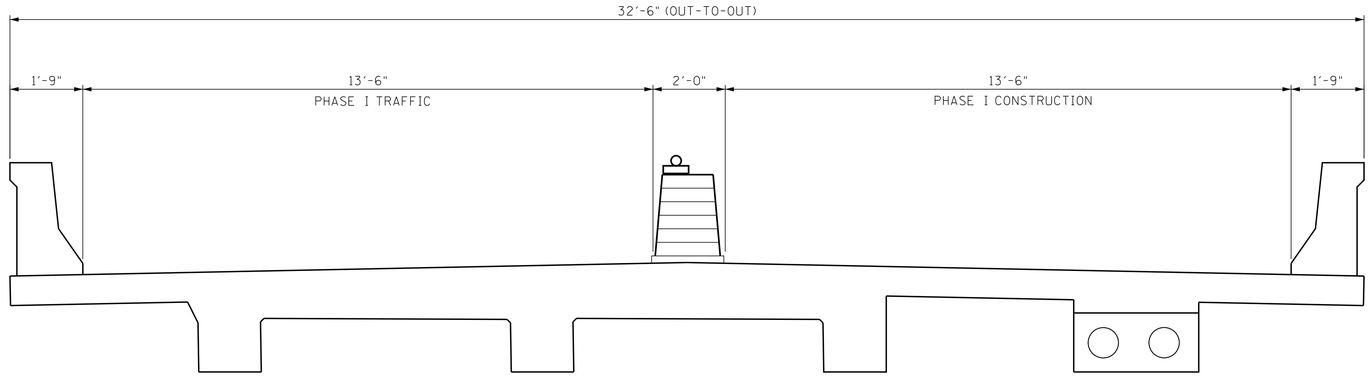
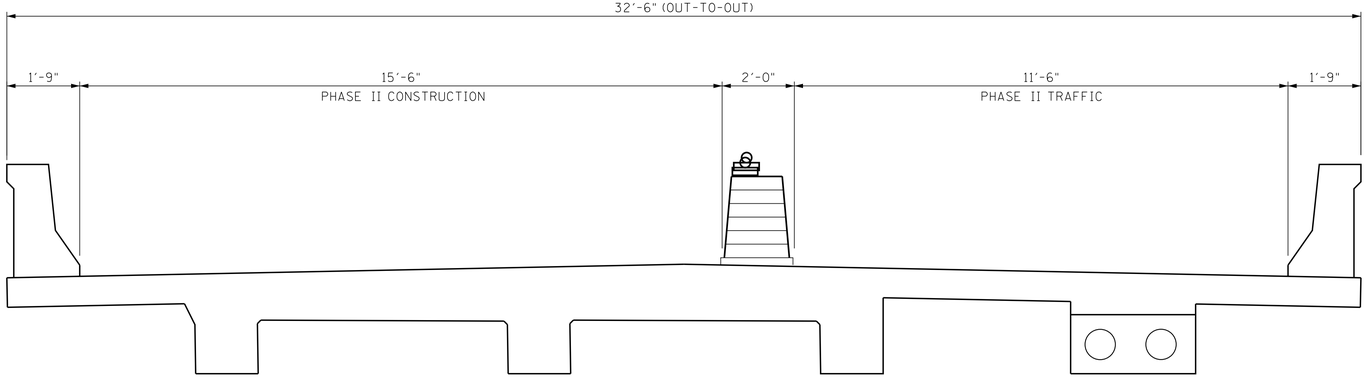



NORTHBOUND BRIDGE

(66-SR22-15.63)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)




NORTHBOUND BRIDGE

(66-SR22-15.63)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
SPANS 2,5,8, & 18
66-SR22-15.63
OVER
NORTH REELFOOT CREEK
BR. NO. 66SR0220009
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 11/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 11/23 |
| CHECKED BY | | DATE | |

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| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B7 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
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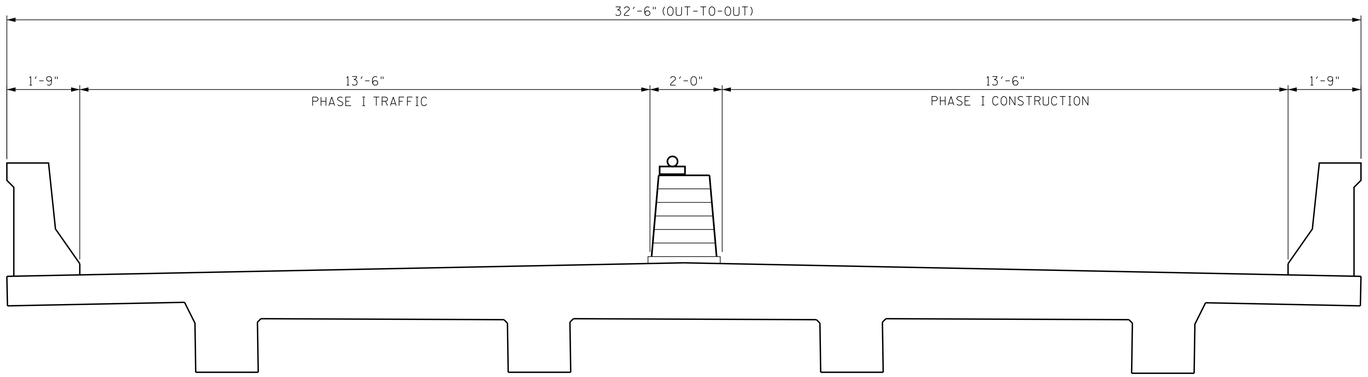
| 1 | 11/14/23 | ZH | ADDED SHEET |

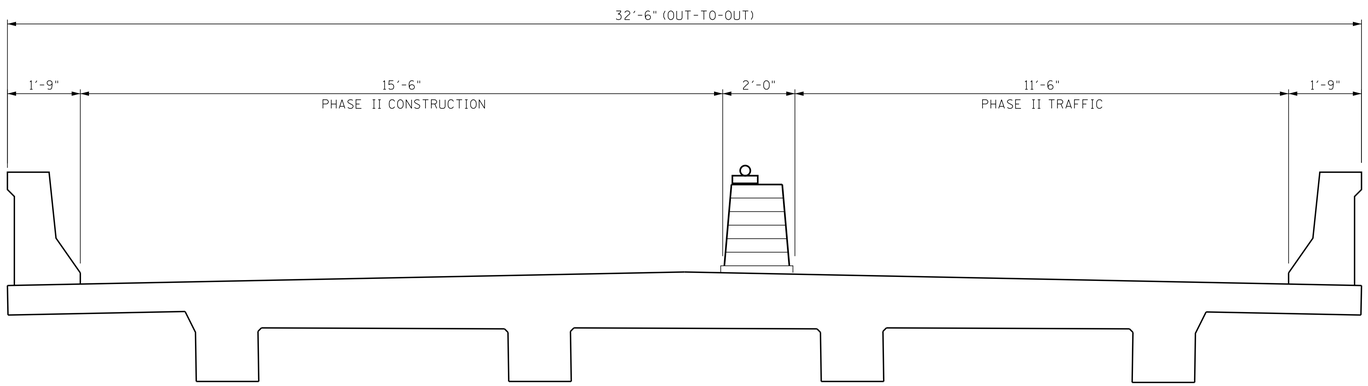



NORTHBOUND BRIDGE

(66-SR22-15.63)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)




NORTHBOUND BRIDGE

(66-SR22-15.63)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
SPANS 9,10,11,16,17,19, & 20
66-SR22-15.63
OVER
NORTH REELFOOT CREEK
BR. NO. 66SR0220009
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 11/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 11/23 |
| CHECKED BY | | DATE | |

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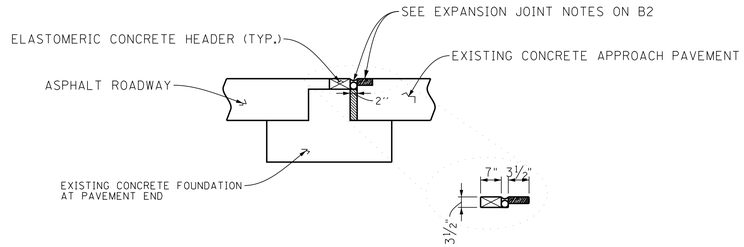
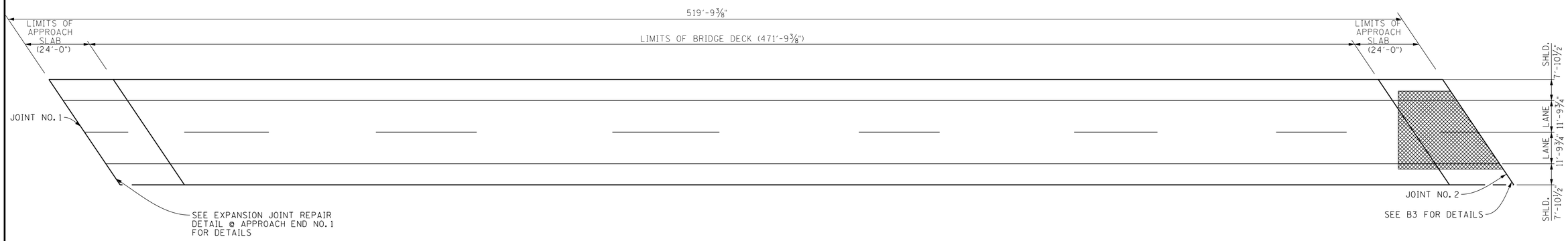
| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B8 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 11/14/23 | ZH | ADDED SHEET |

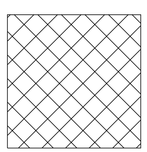



EXPANSION JOINT REPAIR DETAIL
@ APPROACH END NO. 1




PLAN VIEW

BRIDGE ID. NO. 66SR0220015



ASPHALT COVERED

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
66-SR22-18.60
OVER
REELFOOT CREEK
BR. NO. 66SR0220015
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 11/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 11/23 |
| CHECKED BY | | DATE | |



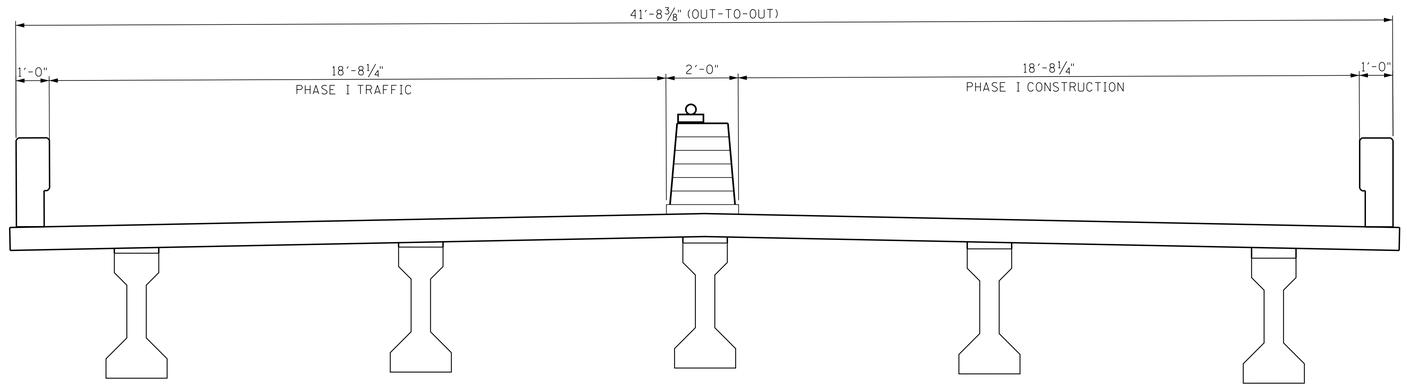
$$$$SYTIME$$$$$$$
$$$$DGNSPEC$$$$$$$

PIN 133812.00

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B9 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 11/14/23 | ZH | ADDED SHEET |

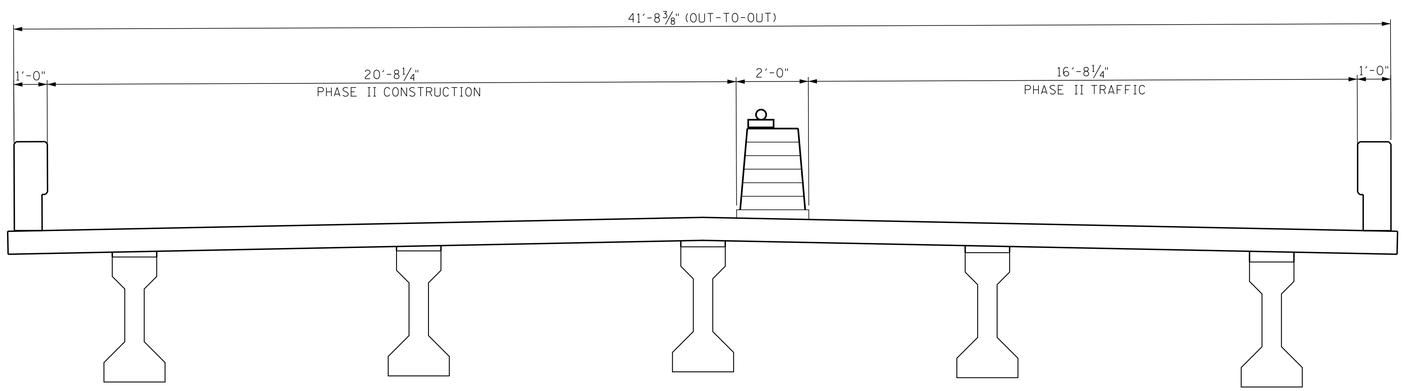



NORTHBOUND BRIDGE

(66-SR22-18.60)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)




NORTHBOUND BRIDGE

(66-SR22-18.60)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

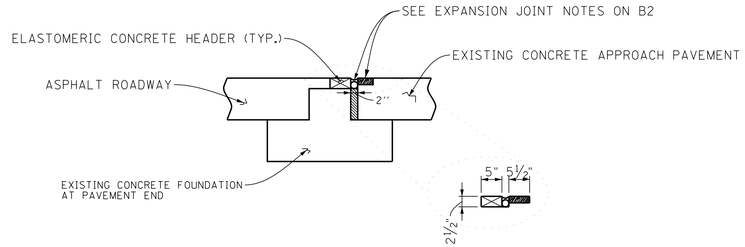
BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
66-SR22-18.60
OVER
REELFOOT CREEK
BR. NO. 66SR0220015
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 11/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 11/23 |
| CHECKED BY | | DATE | |

$$$$SYTIME$$$$$$$
955:06 AM
$$$$DONSPEC$$$$$$$



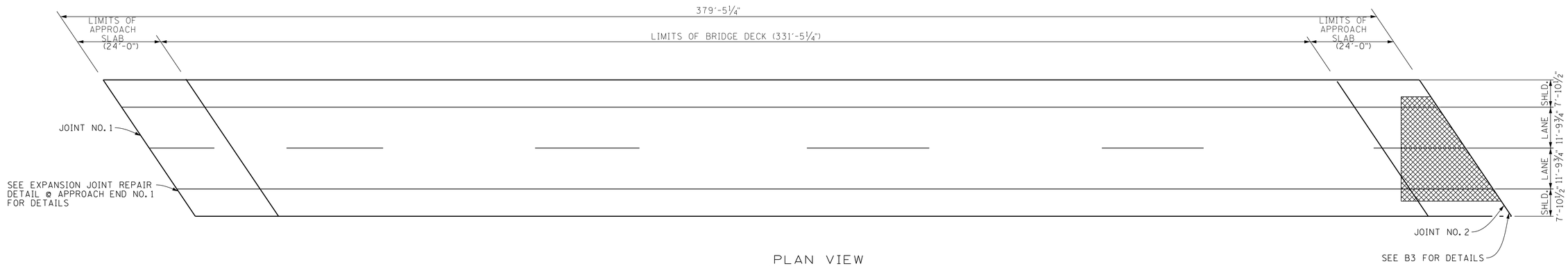

EXPANSION JOINT REPAIR DETAIL
@ APPROACH END NO. 1

PIN 133812.00

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B10 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 11/14/23 | ZH | ADDED SHEET |




PLAN VIEW

BRIDGE ID. NO. 66SR0220017

ASPHALT COVERED

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
66-SR22-18.79
OVER
OVERFLOW
BR. NO. 66SR0220017
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 11/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 11/23 |
| CHECKED BY | | DATE | |



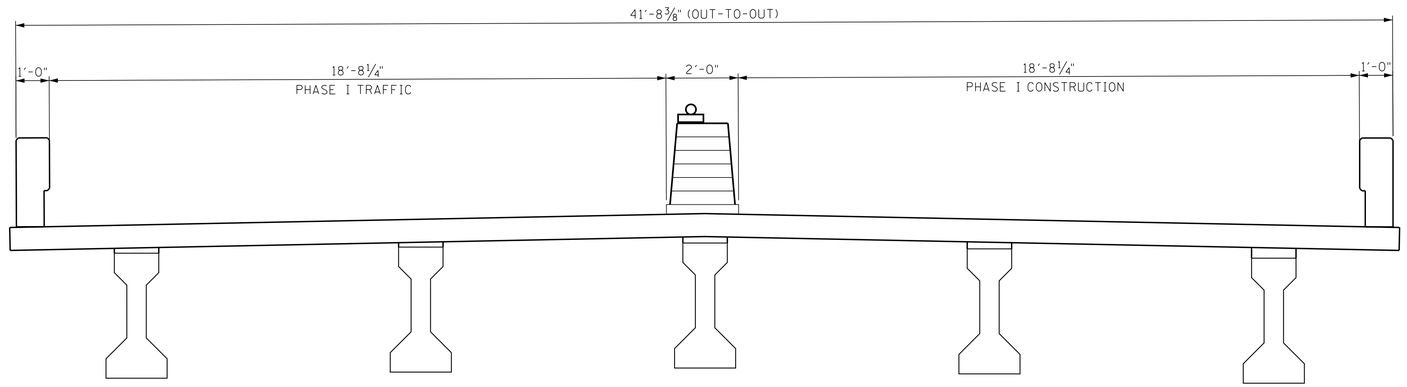
$$$$SYTIME$$$$$$
9:55:30 AM
$$$$DGNSPEC$$$$$$

PIN 133812.00

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B11 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 11/14/23 | ZH | ADDED SHEET |

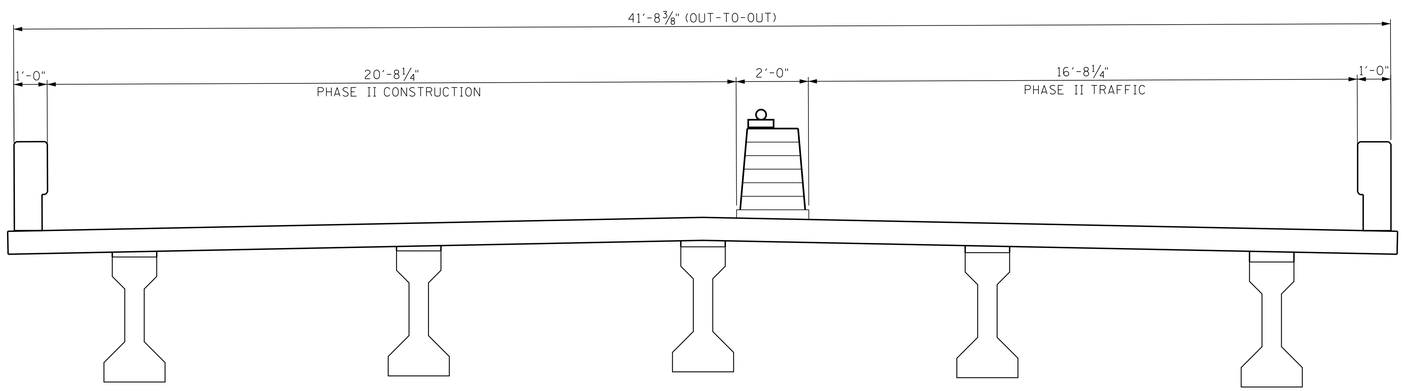



NORTHBOUND BRIDGE

(66-SR22-18.79)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

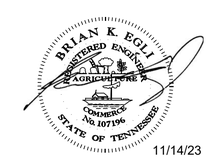



NORTHBOUND BRIDGE

(66-SR22-18.79)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)



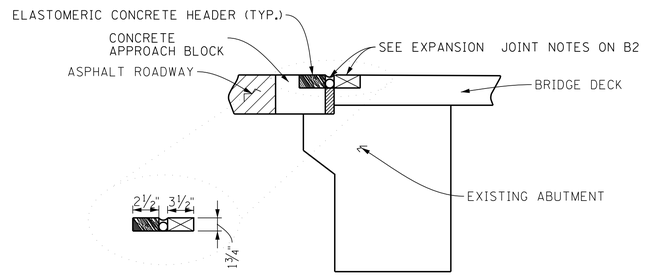

11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

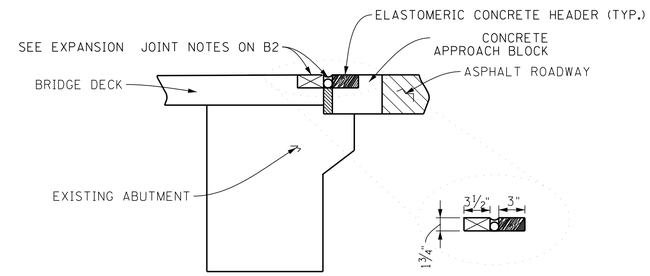
PHASE CONSTRUCTION
66-SR22-18.79
OVER
OVERFLOW
BR. NO. 66SR0220017
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 11/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 11/23 |
| CHECKED BY | | DATE | |

$$$$SYTIME$$$$$$
9:56:00 AM
$$$$DGNSPEC$$$$




EXPANSION JOINT REPAIR DETAIL
@ ABUTMENT NO. 1



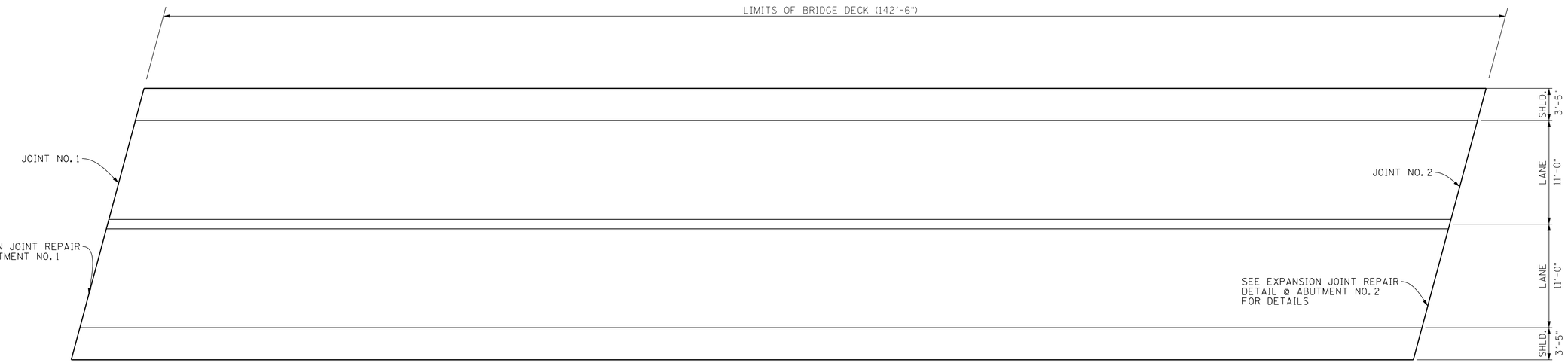

EXPANSION JOINT REPAIR DETAIL
@ ABUTMENT NO. 2

PIN 133812.00

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B12 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 10/11/23 | ZH | REVISED TITLE & YEAR |
| 2 | 11/14/23 | ZH | REVISED TITLE & PAGE NUMBER |




PLAN VIEW

BRIDGE ID. NO. 66SR0220021

11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
66-SR22-23.35
OVER
INDIAN CREEK
BR. NO. 66SR0220021
OBION COUNTY
2024

DESIGNED BY _____ DATE _____
DRAWN BY ZACH HAYNES DATE 6/23
SUPERVISED BY KEVIN MARTINKO DATE 6/23
CHECKED BY _____ DATE _____

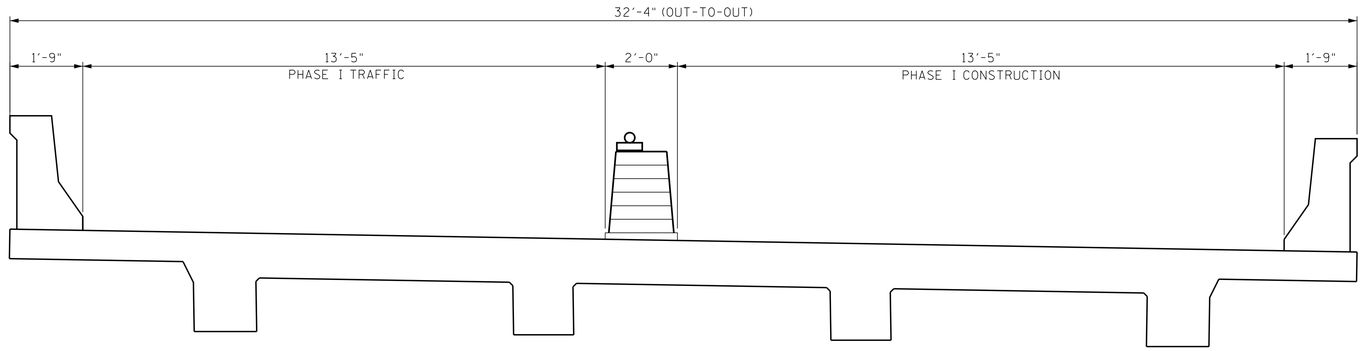
$$$$SYTIME$$$$$$
95653.AM
$$$$DGNSPEC$$$$$$

PIN 133812.00

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B13 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 10/11/23 | ZH | REVISED TITLE & YEAR |
| 2 | 11/14/23 | ZH | REVISED TITLE & PAGE NUMBER |

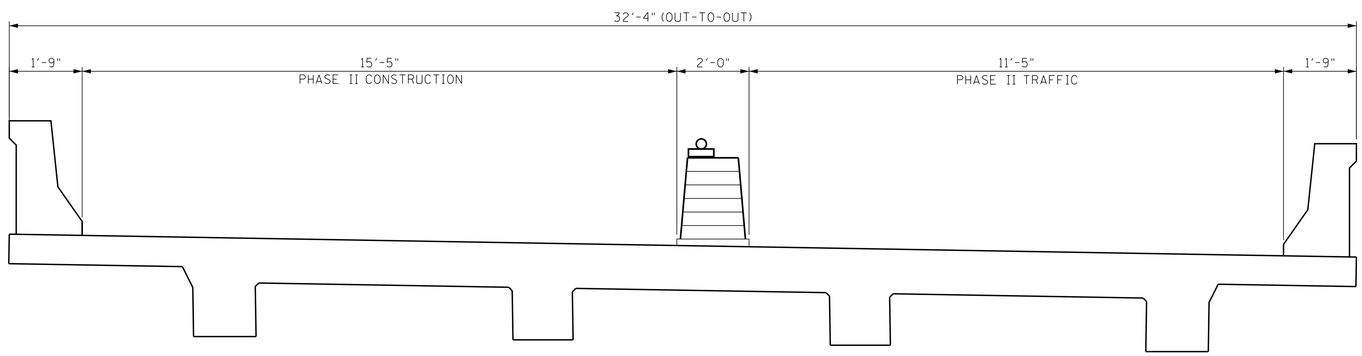



NORTHBOUND BRIDGE

(66-SR22-23.35)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)




NORTHBOUND BRIDGE

(66-SR22-23.35)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
SPANS 1, 2, 4, & 5
66-SR22-23.35
OVER
INDIAN CREEK
BR. NO. 66SR0220021
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 6/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 6/23 |
| CHECKED BY | | DATE | |

$$$$SYTIME$$$$$$$
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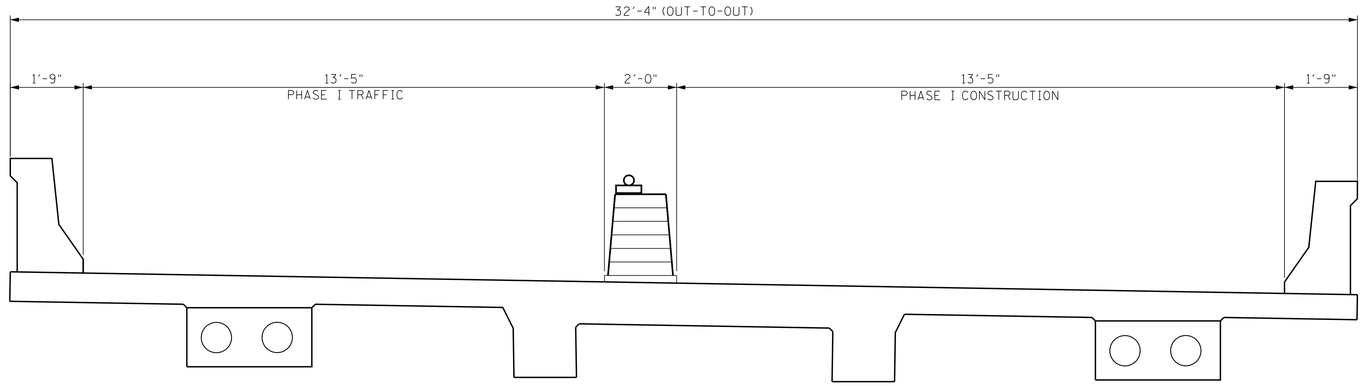
| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66S022-M3-006 | 2024 | B14 |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 10/11/23 | ZH | REVISED TITLE & YEAR |
| 2 | 11/14/23 | ZH | REVISED TITLE & PAGE NUMBER |

PIN 133812.00

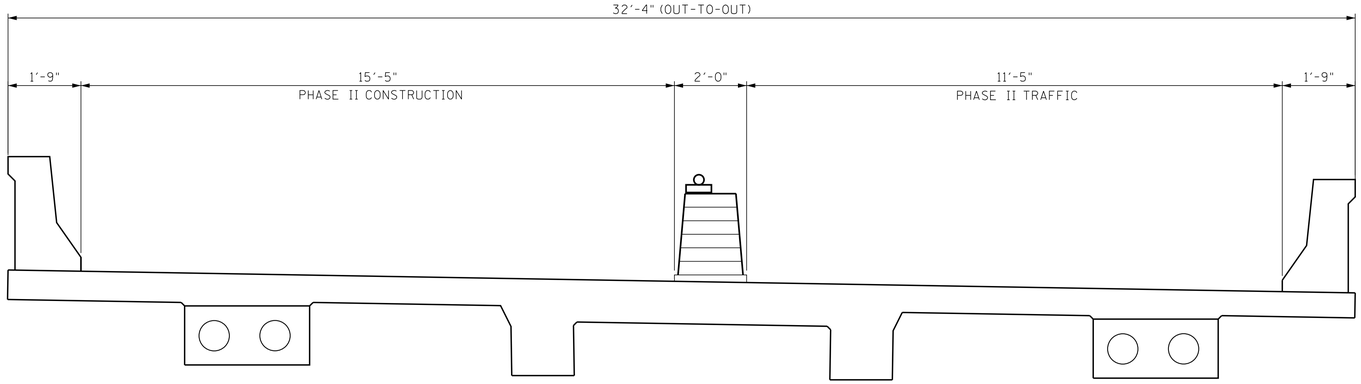



NORTHBOUND BRIDGE

(66-SR22-23.35)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)




NORTHBOUND BRIDGE

(66-SR22-23.35)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE & COMMERCE
No. 107196
STATE OF TENNESSEE
11/14/23

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
SPAN 3
66-SR22-23.35
OVER
INDIAN CREEK
BR. NO. 66SR0220021
OBION COUNTY
2024

| | | | |
|---|---|---|---|
| DESIGNED BY | | DATE | |
| DRAWN BY | ZACH HAYNES | DATE | 6/23 |
| SUPERVISED BY | KEVIN MARTINKO | DATE | 6/23 |
| CHECKED BY | | DATE | |

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9:57:46 AM
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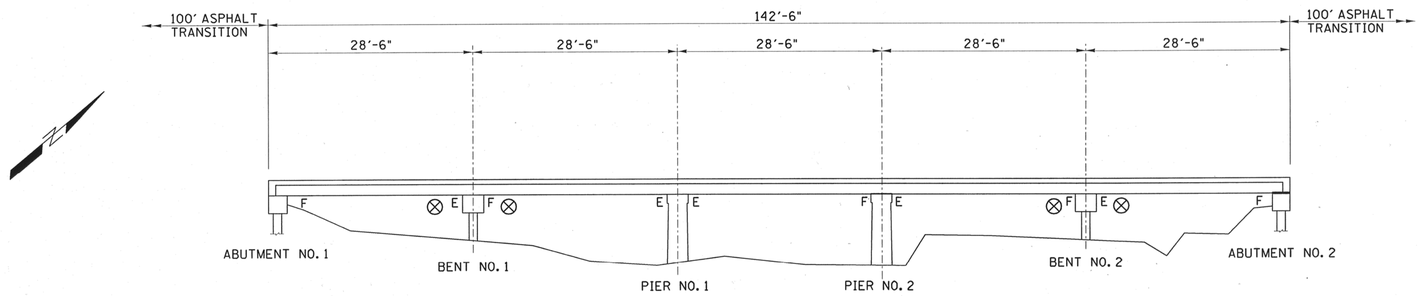


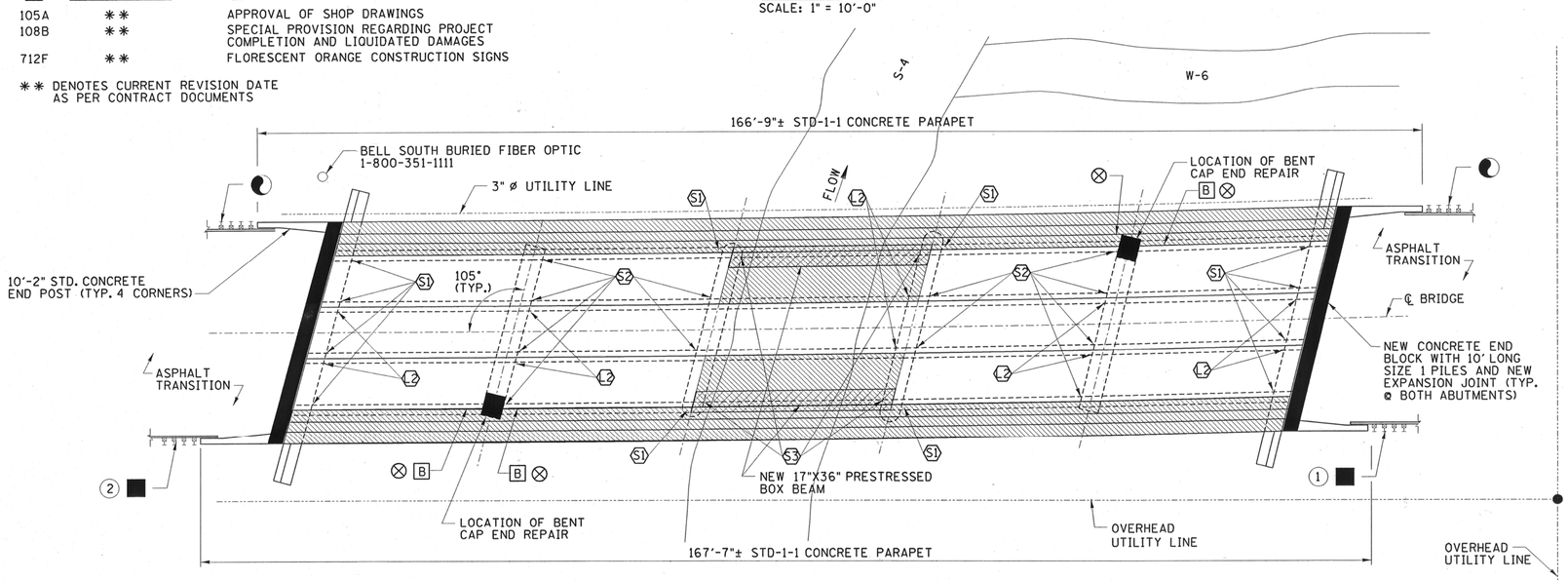

## ELEVATION

SCALE: 1" = 10'-0"

## SPECIAL PROVISIONS

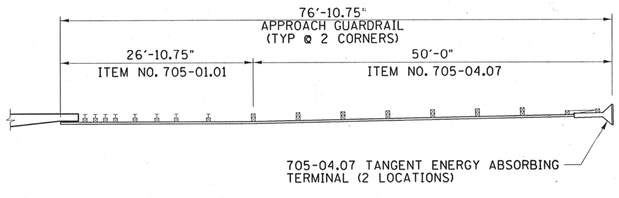
| NO. | LAST REV. DATE | REGARDING |
|---|---|---|
| 105A | ** | APPROVAL OF SHOP DRAWINGS |
| 108B | ** | SPECIAL PROVISION REGARDING PROJECT COMPLETION AND LIQUIDATED DAMAGES |
| 712F | ** | FLORESCENT ORANGE CONSTRUCTION SIGNS |

** DENOTES CURRENT REVISION DATE AS PER CONTRACT DOCUMENTS

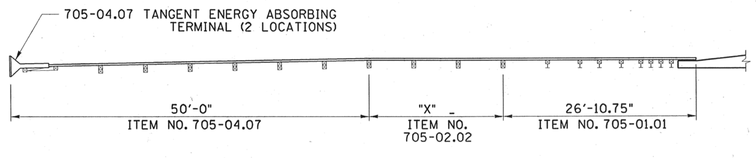



## PLAN

SCALE: 1" = 10'-0"




## DETAIL




| | X |
|---|---|
| 1 | 80' |
| 2 | 110' |

## DETAIL

(TYP. @ 2 CORNERS)

## LIST OF DRAWINGS

| DWG. NO. | REVISION | DRAWING |
|---|---|---|
| BR-88-05 | | LAYOUT OF BRIDGE TO BE REPAIRED (23.35) |
| BR-88-06 | | ESTIMATED QUANTITIES |
| BR-88-07 | | ESTIMATED QUANTITIES |
| BR-88-08 | | GENERAL NOTES |
| BR-88-40 | | BRIDGE REPAIR DETAILS |
| BR-88-41 | | BRIDGE REPAIR DETAILS |
| BR-88-42 | | BRIDGE REPAIR DETAILS |
| BR-88-43 | | BRIDGE REPAIR DETAILS |
| BR-88-44 | | BRIDGE REPAIR DETAILS |
| BR-88-45 | | BRIDGE REPAIR DETAILS |
| BR-88-46 | | BRIDGE REPAIR DETAILS |
| BR-88-47 | | BRIDGE REPAIR DETAILS |
| BR-88-48 | | BRIDGE REPAIR DETAILS |
| BR-88-49 | | BRIDGE REPAIR DETAILS |
| BR-88-50 | | BRIDGE REPAIR DETAILS |
| BR-88-51 | | BRIDGE REPAIR DETAILS |
| BR-88-53 | | BRIDGE REPAIR DETAILS |
| BR-88-54 | | BRIDGE REPAIR DETAILS |
| BR-88-55 | | BRIDGE REPAIR DETAILS |
| BR-88-56 | | BRIDGE REPAIR DETAILS |
| BR-88-59 | | BRIDGE REPAIR DETAILS |

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66012-4213-04 | 2008 | |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |

## SCOPE OF WORK

REMOVE EXISTING ASPHALT OVERLAY AND BINDER COURSE ON BRIDGE DECK. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-40 THRU BR-88-43.

SCARIFY EXISTING BRIDGE DECK 1" AND PROVIDE NEW CONCRETE OVERLAY. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-40 THRU BR-88-43.

REPLACE EXISTING CANTILEVERS, DIAPHRAGMS, PORTION OF EXISTING SLAB, AND DESIGNATED BEAMS. FOR NOTES AND DETAILS, NOS. BR-88-40 THRU BR-88-43.

PROVIDE NEW 17" X 36" PRESTRESSED PRECAST BOX BEAMS TO REPLACE THE BEAMS THAT ARE REMOVED. FOR NOTES AND DETAILS, NOS. BR-88-40 THRU BR-88-43, BR-88-46 AND BR-88-47.

TEXTURE COAT NEW PARAPETS, END POSTS, CANTILEVERS AND EXTERIOR FACE OF NEW OR EXISTING CAST-IN-PLACE BEAMS. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-54.

PROVIDE NEW CONCRETE PARAPET AND STANDARD END POSTS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-40 THRU BR-88-43.

PROVIDE PARTIAL AND FULL DEPTH DECK REPAIR. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-56.

PROVIDE BRIDGE DECK GROOVING TO WITHIN 1'-0" FROM THE FACE OF THE PARAPET. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-40 THRU BR-88-43.

PROVIDE TRAFFIC CONTROL USING CONCRETE BARRIER RAIL AND SIGNAL LIGHTS. KEEP TRAFFIC LANE OPEN ALL THE TIME. FOR NOTES AND DETAILS, SEE SHEET NO. 14.

PROVIDE NEW GUARDRAIL AT ALL FOUR CORNERS OF BRIDGE. FOR NOTES AND DETAILS SEE DETAILS THIS SHEET.

PROVIDE ASPHALT TRANSITION AT EACH END OF BRIDGE. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-59.

REPAIR DETERIORATED BENT CAP ENDS AT DESIGNATED LOCATIONS. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-48.

L2 PROVIDE LATERAL SEISMIC RESTRAINTS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-50 AND BR-88-53.

S1 S2 S3 PROVIDE LONGITUDINAL SEISMIC RESTRAINTS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-50 AND BR-88-51.

PROVIDE CONCRETE END BLOCK AT EACH ABUTMENT WITH 10' LONG SIZE 1 CONCRETE PILES. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-49.

PROVIDE SPALL REPAIR WHERE NEEDED. THE ENGINEER FROM TENNESSEE D.O.T. SHALL LOCATE SPALL REPAIR AREAS. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-54.

B PROVIDE BEAM END REPAIR AT DESIGNATED LOCATIONS. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-38.

⊗ PROVIDE TEMPORARY SUPPORTS AT DESIGNATED LOCATIONS TO FACILITATE BEAM END REPAIR AND BENT CAP REPAIR.

## LIST OF REFERENCE DRAWINGS

| DWG. NO. | DRAWING |
|---|---|
| C-9-108 | LAYOUT OF BRIDGE |
| C-9-109 | SPANS 1-5 |
| C-9-110 | ABUTMENTS & BENTS |
| C-9-111 | DETAILS OF BRIDGE |

BR. NO. 66-SR22-23.35

ALL REFERENCE DRAWINGS TO BE PRINTED WITH THE PLANS

STATE OF TENNESSEE
**DEPARTMENT OF TRANSPORTATION**

LAYOUT OF BRIDGE TO BE REPAIRED

**SR22 OVER INDIAN CREEK BRIDGE NO. 66-SR22-23.35**

**OBION COUNTY**
**2008**

AMJAD S. KHAIRI
REGISTERED ENGINEER
AUTHORIZED
TENNESSEE
NO. 106600
STATE OF TENNESSEE
1/8/2008

**GARVER ENGINEERS**

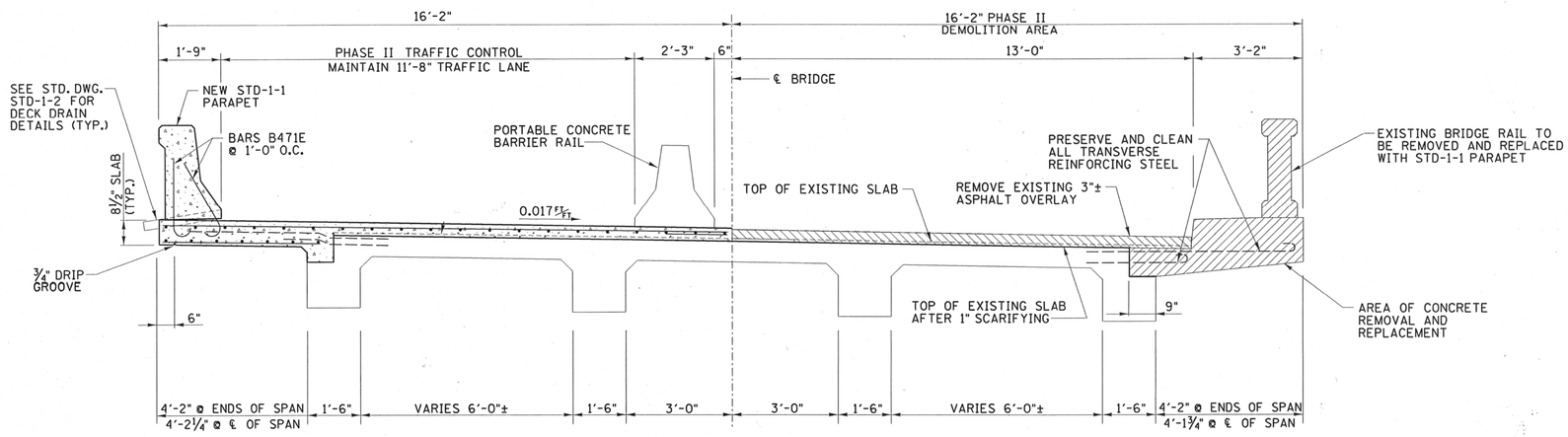
DESIGNED BY A.J. KHAIRI DATE AUG. 2007
DRAWN BY J.P. DELK DATE AUG. 2007
SUPERVISED BY J.H. RUDDELL DATE AUG. 2007
CHECKED BY A.J. KHAIRI DATE AUG. 2007
TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

jpdelk 1/8/2008 2:11:59 PM
WORKSPACE: TDOT
u:\2006\060555\obion county\Final\LAYOUTINDIANCREEK.dgn

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66012-4213-04 | 2008 | |

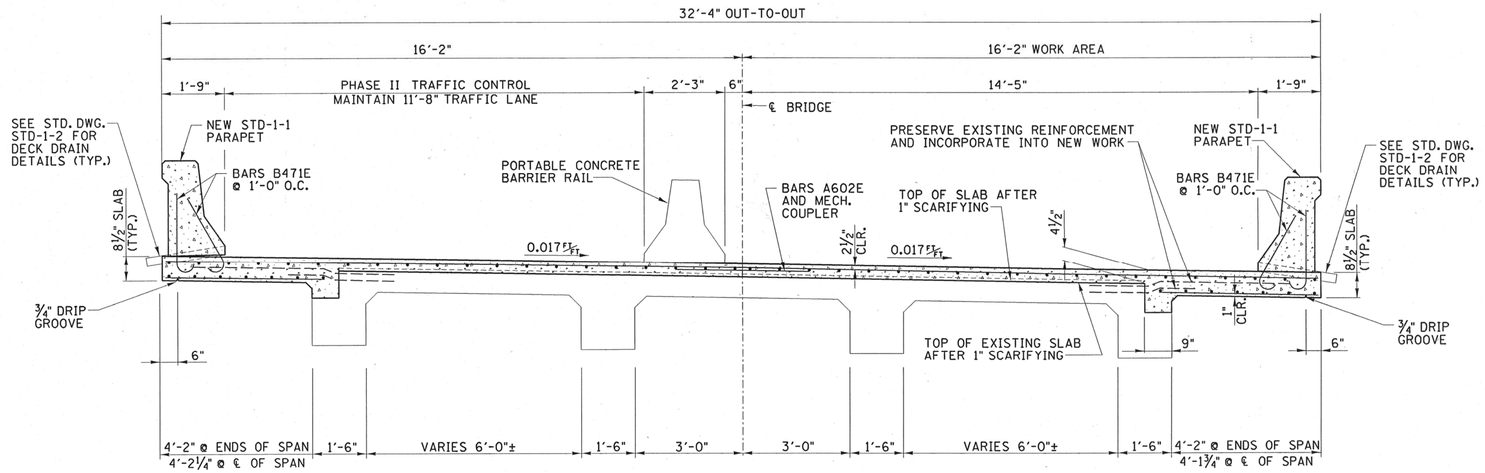
REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |




PHASE II DEMOLITION *

SCALE: ½" = 1'-0"




PHASE II CONSTRUCTION *

SCALE: ½" = 1'-0"

23.35

* TYPICAL SPANS 1, 2, 4, AND 5

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

SR22 OVER INDIAN CREEK
BRIDGE NO. 66-SR22-23.35

OBION COUNTY
2008

BR-88-41

GARVER ENGINEERS

DESIGNED BY A.J. KHAIRI DATE AUG. 2007
DRAWN BY J.P. DELK DATE AUG. 2007
SUPERVISED BY J.H. RUDDELL DATE AUG. 2007
CHECKED BY A.J. KHAIRI DATE AUG. 2007
TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

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WORKSPACE: TDOT
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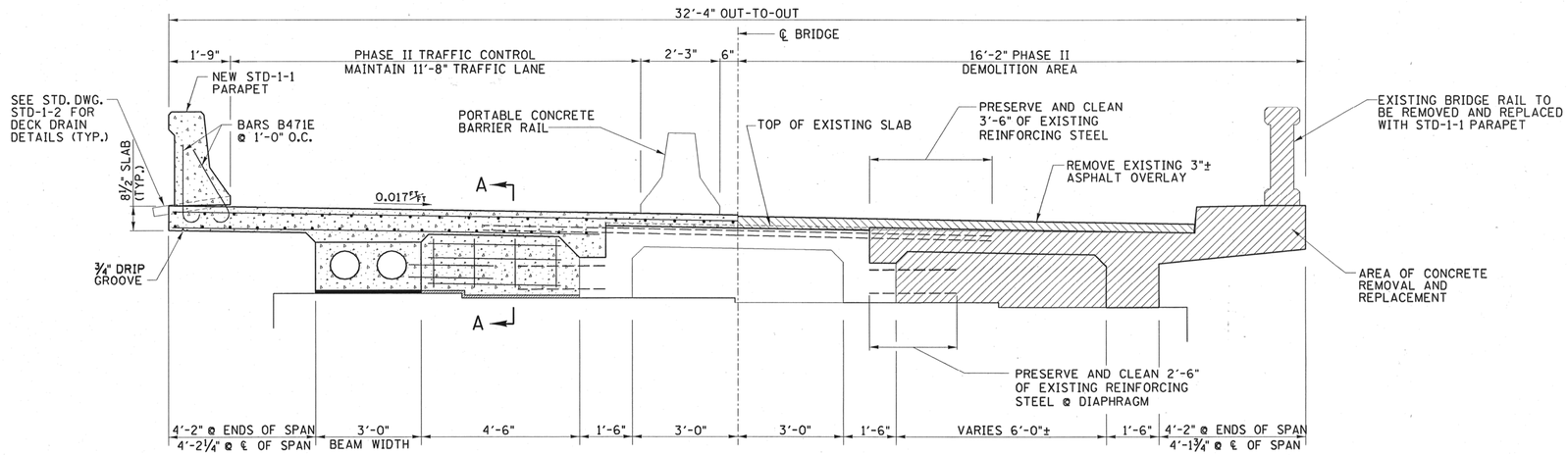
| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66012-4213-04 | 2008 | |

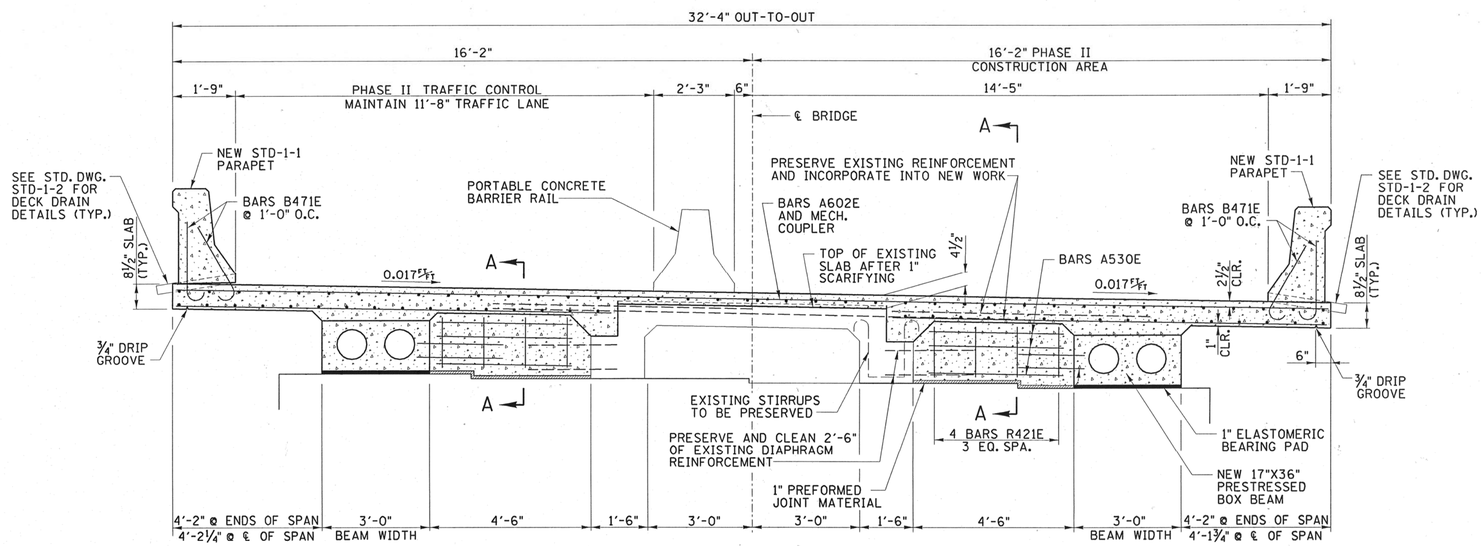
REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |




PHASE II DEMOLITION *

SCALE: ½" = 1'-0"




NOTE:
FOR SECTION A-A, SEE
DWG. NO. BR-88-45

PHASE II CONSTRUCTION *

SCALE: ½" = 1'-0"

* TYPICAL SPAN 3 ONLY

23.35

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

SR22 OVER INDIAN CREEK
BRIDGE NO. 66-SR22-23.35

OBION COUNTY
2008

BR-88-43

GARVER ENGINEERS

| | | | |
|---|---|---|---|
| DESIGNED BY | A.J. KHAIRI | DATE | AUG. 2007 |
| DRAWN BY | J.P. DELK | DATE | AUG. 2007 |
| SUPERVISED BY | J.H. RUDDELL | DATE | AUG. 2007 |
| CHECKED BY | A.J. KHAIRI | DATE | AUG. 2007 |

TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

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WORKSPACE: TDOT
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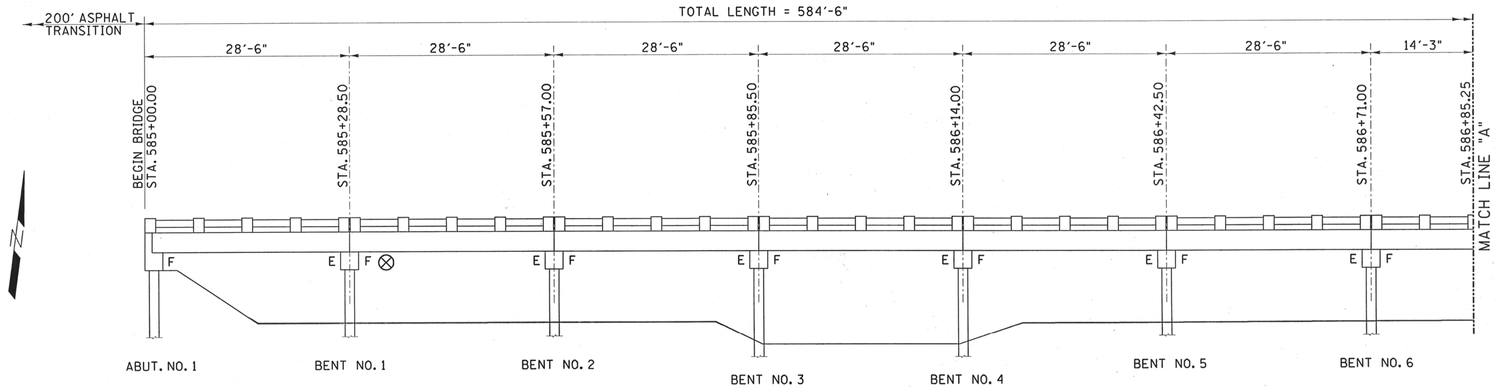
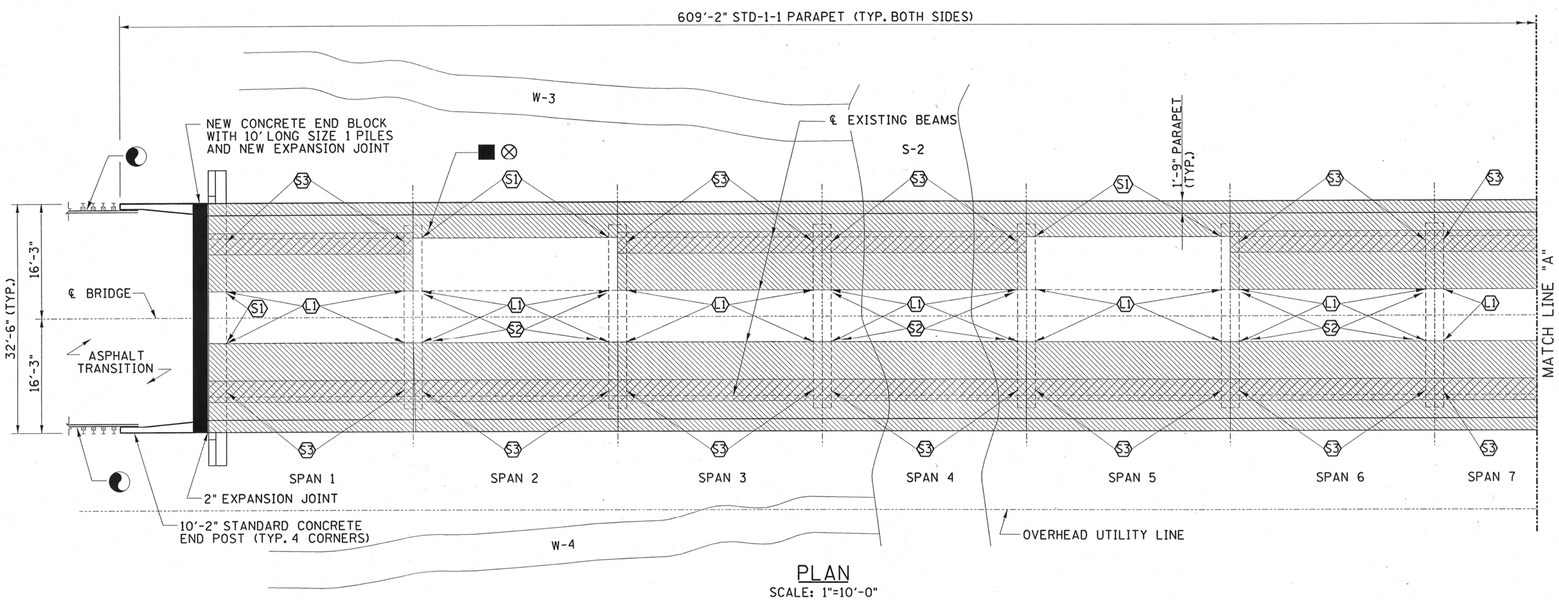


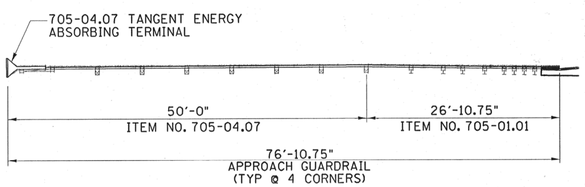

## ELEVATION

SCALE: 1"=10'-0"




## PLAN

SCALE: 1"=10'-0"




## DETAIL

## LIST OF DRAWINGS

| DWG. NO. | REVISION | DRAWING |
|---|---|---|
| BR-88-03 | | LAYOUT OF BRIDGE TO BE REPAIRED (15.63) |
| BR-88-04 | | LAYOUT OF BRIDGE TO BE REPAIRED (15.63) |
| BR-88-06 | | ESTIMATED QUANTITIES |
| BR-88-07 | | ESTIMATED QUANTITIES |
| BR-88-08 | | GENERAL NOTES |
| BR-88-19 | | BRIDGE REPAIR DETAILS |
| BR-88-20 | | BRIDGE REPAIR DETAILS |
| BR-88-22 | | BRIDGE REPAIR DETAILS |
| BR-88-23 | | BRIDGE REPAIR DETAILS |
| BR-88-24 | | BRIDGE REPAIR DETAILS |
| BR-88-25 | | BRIDGE REPAIR DETAILS |
| BR-88-26 | | BRIDGE REPAIR DETAILS |
| BR-88-27 | | BRIDGE REPAIR DETAILS |
| BR-88-28 | | BRIDGE REPAIR DETAILS |
| BR-88-29 | | BRIDGE REPAIR DETAILS |
| BR-88-30 | | BRIDGE REPAIR DETAILS |
| BR-88-31 | | BRIDGE REPAIR DETAILS |
| BR-88-32 | | BRIDGE REPAIR DETAILS |
| BR-88-33 | | BRIDGE REPAIR DETAILS |
| BR-88-34 | | BRIDGE REPAIR DETAILS |
| BR-88-35 | | BRIDGE REPAIR DETAILS |
| BR-88-36 | | BRIDGE REPAIR DETAILS |
| BR-88-37 | | BRIDGE REPAIR DETAILS |
| BR-88-38 | | BRIDGE REPAIR DETAILS |
| BR-88-39 | | BRIDGE REPAIR DETAILS |
| BR-88-50 | | BRIDGE REPAIR DETAILS |
| BR-88-51 | | BRIDGE REPAIR DETAILS |
| BR-88-52 | | BRIDGE REPAIR DETAILS |
| BR-88-53 | | BRIDGE REPAIR DETAILS |
| BR-88-54 | | BRIDGE REPAIR DETAILS |
| BR-88-55 | | BRIDGE REPAIR DETAILS |
| BR-88-56 | | BRIDGE REPAIR DETAILS |
| BR-88-59 | | BRIDGE REPAIR DETAILS |

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66012-4213-04 | 2008 | |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |

## SCOPE OF WORK

REMOVE EXISTING ASPHALT OVERLAY AND BINDER COURSE ON BRIDGE DECK. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-22 THRU BR-88-29 AND DWG. NO. BR-88-59.

SCARIFY EXISTING BRIDGE DECK 1" AND PROVIDE NEW CONCRETE OVERLAY. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-22 THRU BR-88-29.

REPLACE EXISTING CANTILEVERS, DIAPHRAGMS, DESIGNATED PORTION OF SLAB AND DESIGNATED BEAMS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-22 THRU BR-88-29.

PROVIDE NEW 17" X 36" PRESTRESSED PRECAST BOX BEAMS TO REPLACE THE BEAMS THAT ARE REMOVED. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-19 AND BR-88-22 THRU BR-88-29.

TEXTURE COAT NEW PARAPETS, END POSTS, CANTILEVERS AND EXTERIOR FACE OF NEW OR EXISTING CAST-IN-PLACE BEAMS. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-54.

PROVIDE NEW CONCRETE PARAPET AND STANDARD END POSTS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-22 THRU BR-88-29.

PROVIDE PARTIAL AND FULL DEPTH DECK REPAIR. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-56.

PROVIDE BRIDGE DECK GROOVING TO WITHIN 1'-0" FROM THE FACE OF THE PARAPET. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-22 THRU BR-88-29.

PROVIDE TRAFFIC CONTROL USING CONCRETE BARRIER RAIL AND SIGNAL LIGHTS. KEEP ONE TRAFFIC LANE OPEN ALL THE TIME. FOR NOTES AND DETAILS, SEE SHEET 13.

PROVIDE NEW GUARDRAIL AT ALL FOUR CORNERS OF BRIDGE. FOR NOTES, SEE DETAILS THIS SHEET.

PROVIDE ASPHALT TRANSITION AT EACH END OF BRIDGE. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-59.

● REPAIR DETERIORATED BENT CAP ENDS AT DESIGNATED LOCATIONS. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-20.

REMOVE EXISTING ROCKER BEARINGS AT PIER NO. 2 AND PROVIDE NEW ELASTOMERIC BEARINGS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-34 THRU BR-88-37.

* PROVIDE NEW STRIP SEAL EXPANSION JOINT AT PIER NO. 2. ADJUST EXISTING DIAPHRAGMS AND BEAM ENDS TO ALLOW FOR THE INSTALLATION OF NEW BEAM. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-35 AND BR-88-36.

L1 L2 PROVIDE LATERAL SEISMIC RESTRAINTS AT ALL BENTS AND ABUTMENTS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-50, BR-88-52 AND BR-88-53.

S1 S2 S3 PROVIDE LONGITUDINAL SEISMIC RESTRAINTS AT ALL BENTS AND ABUTMENTS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-50 AND BR-88-51.

PROVIDE CONCRETE END BLOCKS AT EACH ABUTMENT WITH 10' LONG SIZE 1 CONCRETE PILES. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-39.

PROVIDE NEW EXPANSION JOINT AT EACH ABUTMENT. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-55.

PROVIDE SPALL REPAIR WHERE NEEDED. THE ENGINEER FROM TENNESSEE D.O.T. SHALL LOCATE SPALL REPAIR AREAS. FOR NOTES AND DETAILS, SEE DWG. NO. BR-88-54.

■ PROVIDE BEAM END REPAIR AT DESIGNATED LOCATIONS. FOR NOTES AND DETAILS, SEE DWG. NOS. BR-88-38.

⊗ PROVIDE TEMPORARY SUPPORTS AT DESIGNATED LOCATIONS TO FACILITATE BEAM END REPAIR AND BEARING REPLACEMENT.

## LIST OF SPECIAL PROVISIONS

| DWG. NO. | LAST REV. DATE | REGARDING |
|---|---|---|
| 105A | ** | APPROVAL OF SHOP DRAWINGS |
| 108B | ** | SPECIAL PROVISION REGARDING PROJECT COMPLETION AND LIQUIDATED DAMAGES |
| 604S | 6-20-03 | STRIP SEAL EXPANSION JOINTS |
| 712F | ** | FLORESCENT ORANGE CONSTRUCTION SIGNS |

** DENOTES CURRENT REVISION DATE AS PER CONTRACT DOCUMENTS

## LIST OF REFERENCE DRAWINGS

| DWG. NO. | DRAWING | |
|---|---|---|
| E-14-90 | LAYOUT OF BRIDGE | BR. NO. 66-SR22-15.63 |
| E-4-74 | STANDARD CONCRETE BRIDGES | |
| E-8-131 | STANDARD CONCRETE BRIDGES | |
| E-4-132 | STANDARD ABUT. & BENT | |
| A-0-12 | STANDARD CONCRETE PIERS | |

ALL REFERENCE DRAWINGS TO BE PRINTED WITH THE PLANS

STATE OF TENNESSEE
**DEPARTMENT OF TRANSPORTATION**

LAYOUT OF BRIDGE TO BE REPAIRED

**SR22 OVER NORTH REELFOOT CREEK BRIDGE NO. 66-SR22-15.63**

**OBION COUNTY**
**2008**

**GARVER ENGINEERS**

| | | | |
|---|---|---|---|
| DESIGNED BY | S.F. HARPER | DATE | AUG. 2007 |
| DRAWN BY | J.P. DELK | DATE | AUG. 2007 |
| SUPERVISED BY | J.H. RUDDELL | DATE | AUG. 2007 |
| CHECKED BY | A.J. KHAIRI | DATE | AUG. 2007 |

TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

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WORKSPACE: TDOT
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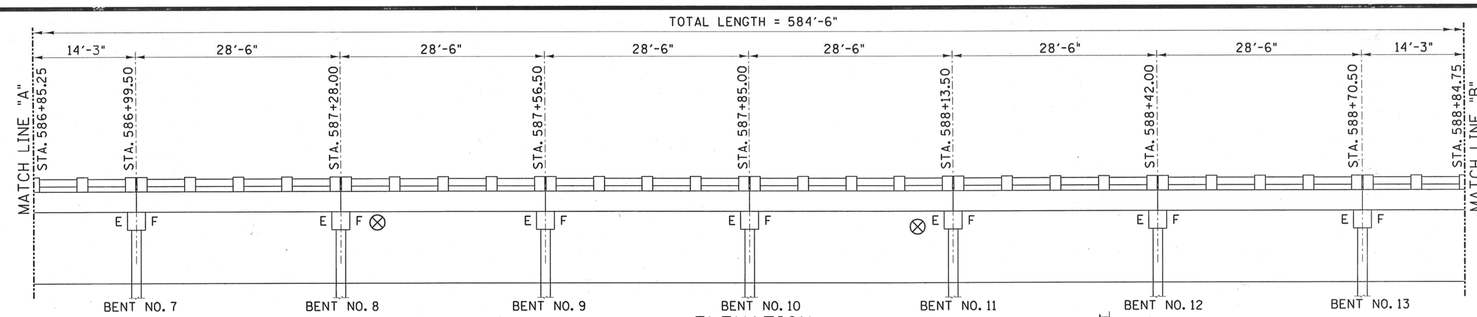
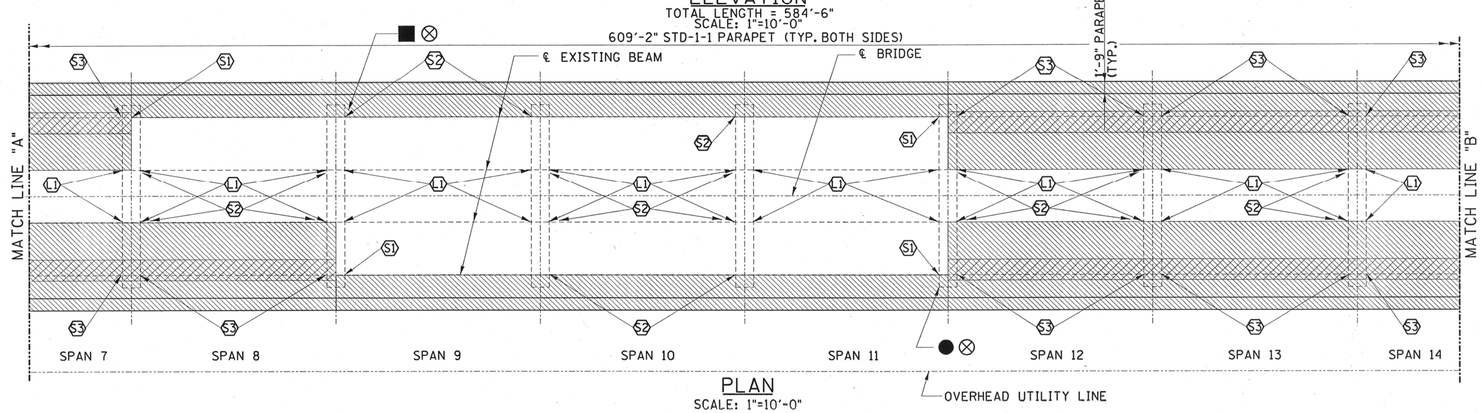
| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66012-4213-04 | 2008 | |

REVISIONS

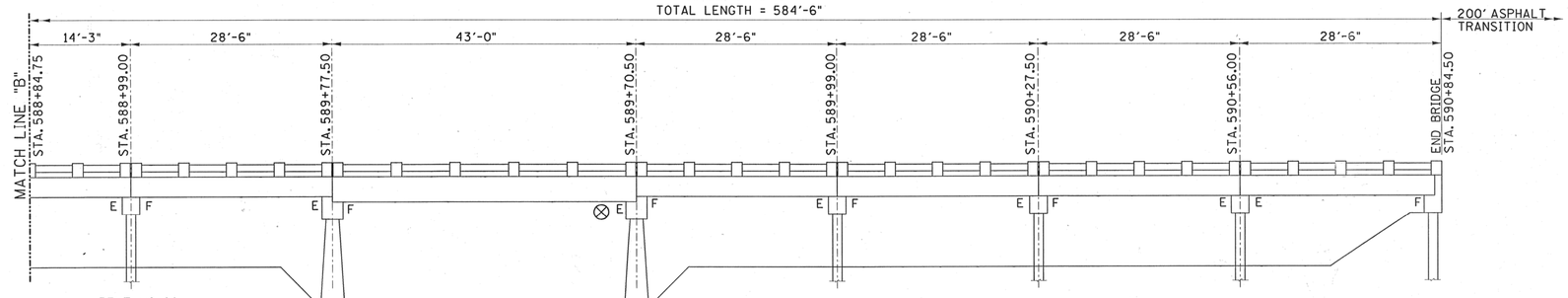
| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |



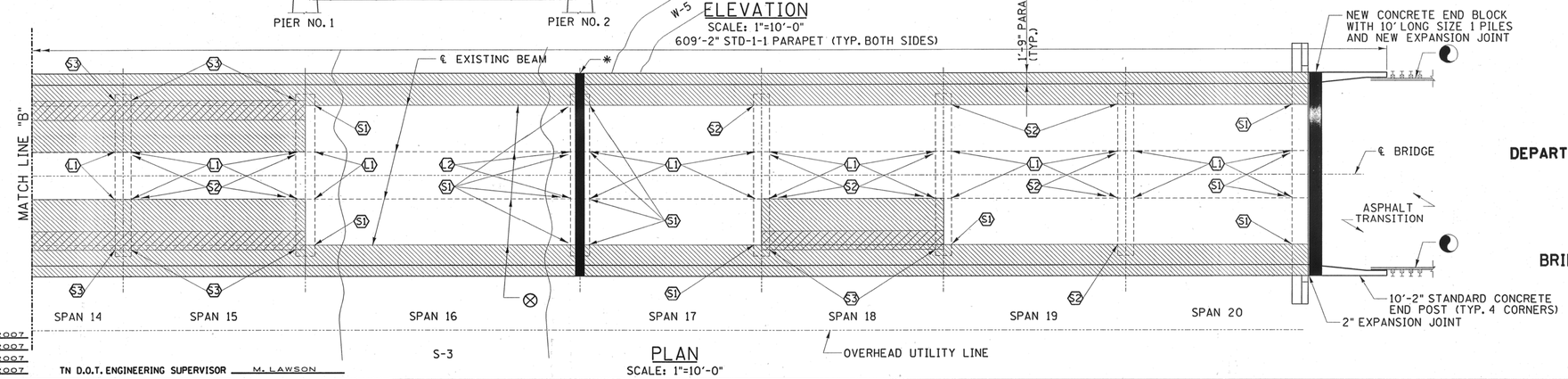

ELEVATION
TOTAL LENGTH = 584'-6"
SCALE: 1"=10'-0"




PLAN
SCALE: 1"=10'-0"




ELEVATION
SCALE: 1"=10'-0"




PLAN
SCALE: 1"=10'-0"

S-3

STATE OF TENNESSEE
**DEPARTMENT OF TRANSPORTATION**

LAYOUT OF BRIDGE
TO BE REPAIRED

**SR22 OVER NORTH REELFOOT CREEK**
**BRIDGE NO. 66-SR22-15.63**

**OBION COUNTY**
**2008**

**GARVER ENGINEERS**

| | | | |
|---|---|---|---|
| DESIGNED BY | S.F. HARPER | DATE | AUG. 2007 |
| DRAWN BY | J.P. DELK | DATE | AUG. 2007 |
| SUPERVISED BY | J.H. RUDDELL | DATE | AUG. 2007 |
| CHECKED BY | A.J. KHAIRI | DATE | AUG. 2007 |

TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

BR-88-04

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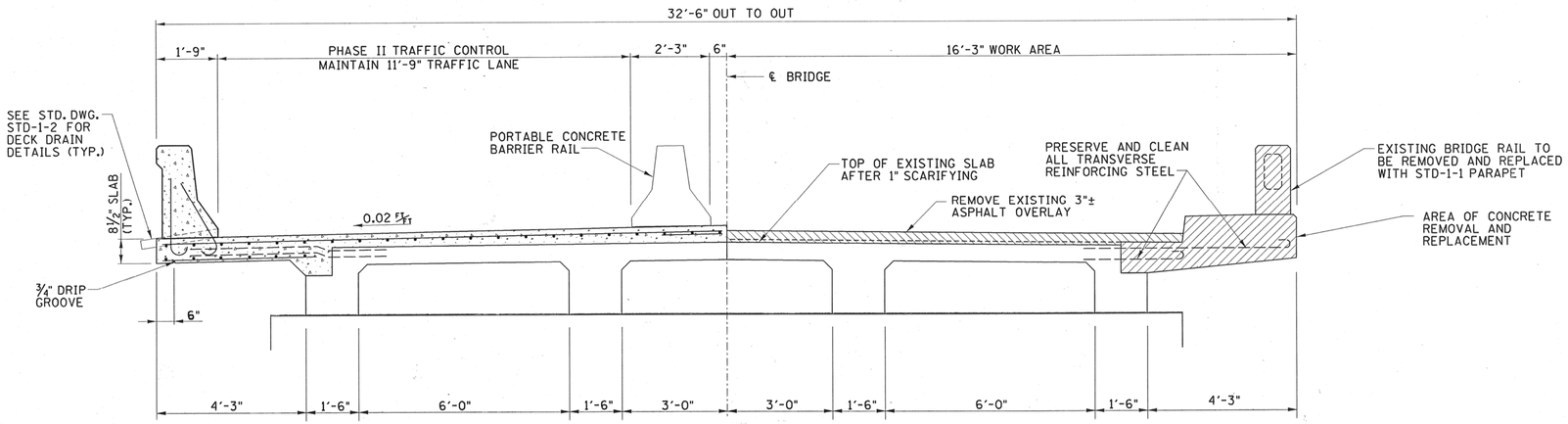
| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66012-4213-04 | 2008 | |

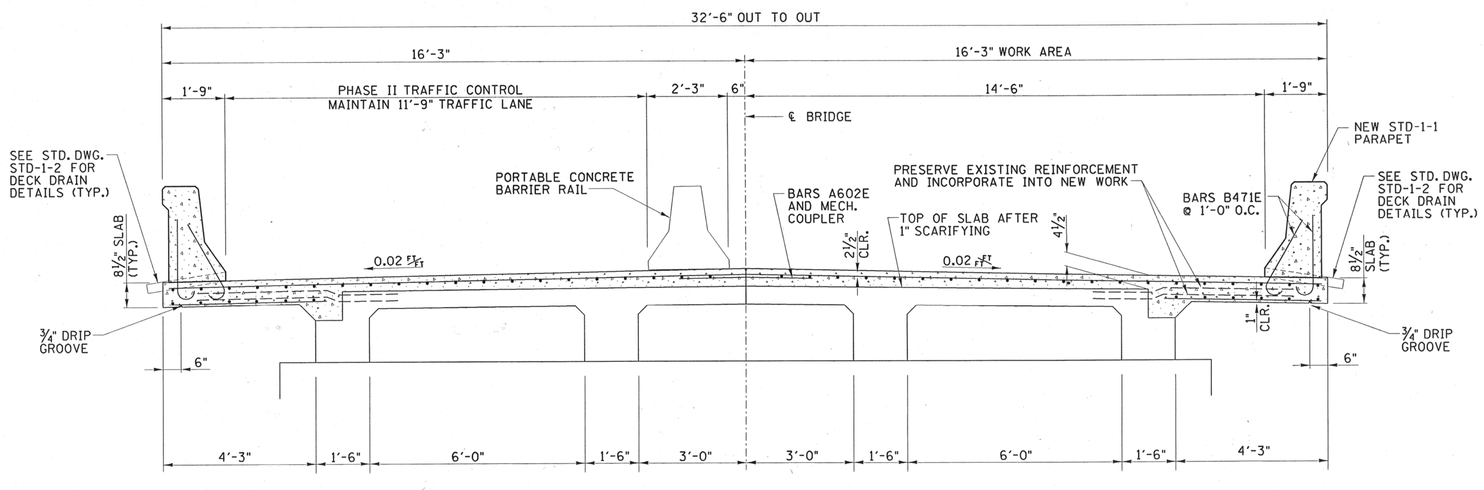
REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |




## PHASE II DEMOLITION ✱

(REPLACE CANTILEVER ON BOTH SIDES)
SCALE: ½" = 1'-0"




## PHASE II CONSTRUCTION ✱

(REPLACE CANTILEVER ON BOTH SIDES)
SCALE: ½" = 1'-0"

✱TYPICAL SPANS 9, 10, 11, 17, 19 AND 20

15.63

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

SR22 OVER NORTH REELFOOT CREEK
BRIDGE NO. 66-SR22-15.63

OBION COUNTY
2008

BR-88-23

**GARVER ENGINEERS**

DESIGNED BY S. F. HARPER DATE AUG. 2007
DRAWN BY J. P. DELK DATE AUG. 2007
SUPERVISED BY J. H. RUDDELL DATE AUG. 2007
CHECKED BY A. J. KHAIRI DATE AUG. 2007

TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

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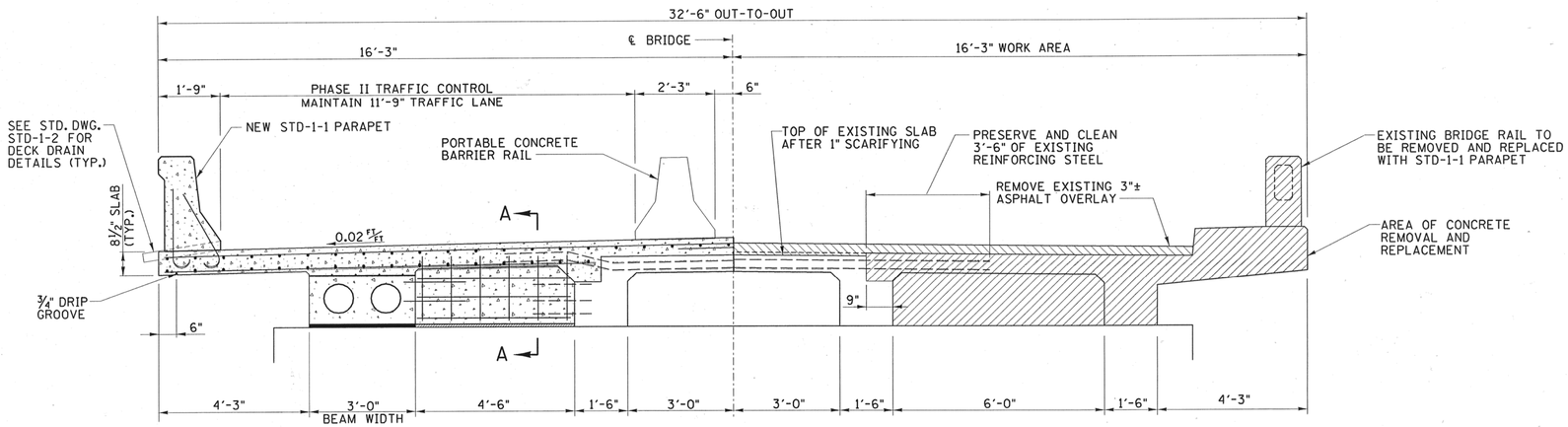
| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66012-4213-04 | 2008 | |

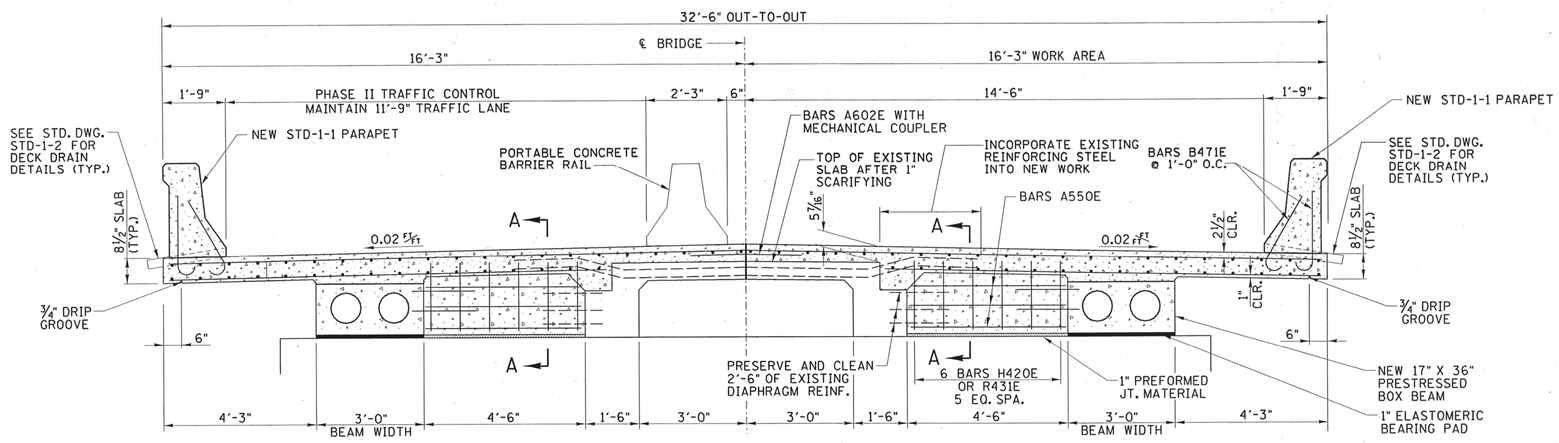
REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |




## PHASE II DEMOLITION ✳

(REPLACE EXTERIOR BEAMS AND CANTILEVERS BOTH SIDES)
SCALE: ½" = 1'-0"




## PHASE II CONSTRUCTION ✳

(REPLACE EXTERIOR BEAMS AND CANTILEVERS BOTH SIDES)
SCALE: ½" = 1'-0"

NOTE:
FOR SECTION A-A, SEE DWG. NO. BR-88-33.

✳ TYPICAL SPANS 1, 3, 4, 6, 7, 12, 13, 14 AND 15

15.63

STATE OF TENNESSEE
**DEPARTMENT OF TRANSPORTATION**

BRIDGE REPAIR DETAILS

SR22 OVER NORTH REELFOOT CREEK
BRIDGE NO. 66-SR22-15.63

OBION COUNTY
2008

BR-88-25

**GARVER ENGINEERS**

DESIGNED BY S. F. HARPER DATE AUG. 2007
DRAWN BY J. P. DELK DATE AUG. 2007
SUPERVISED BY J. H. RUDDELL DATE AUG. 2007
CHECKED BY A. J. KHAIRI DATE AUG. 2007
TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

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| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| 66012-4213-04 | 2008 | |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
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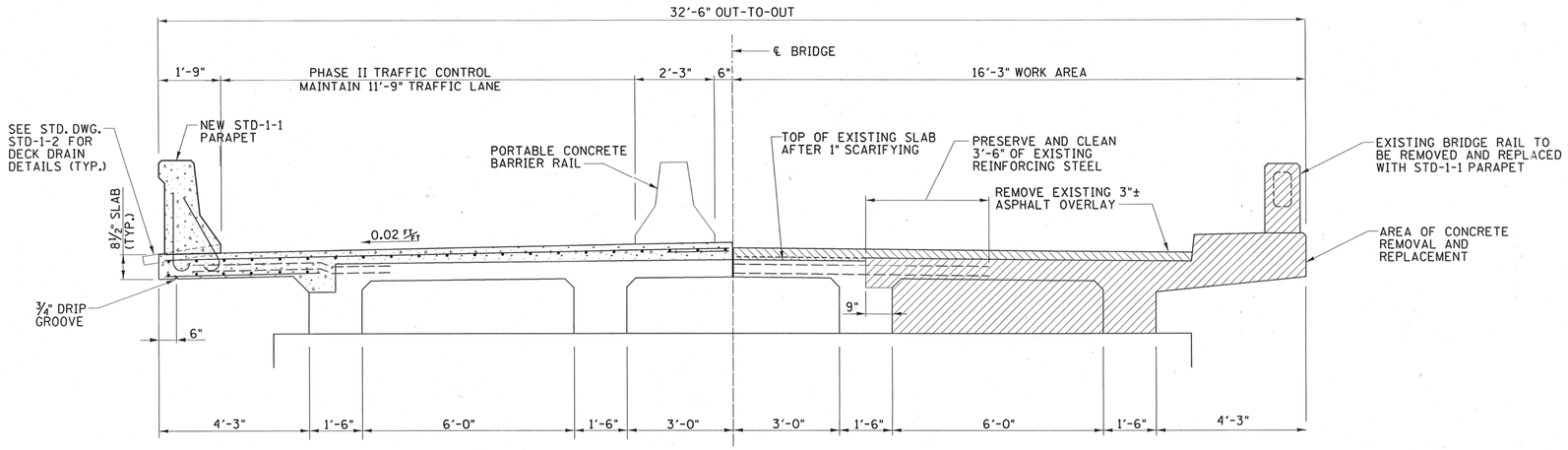
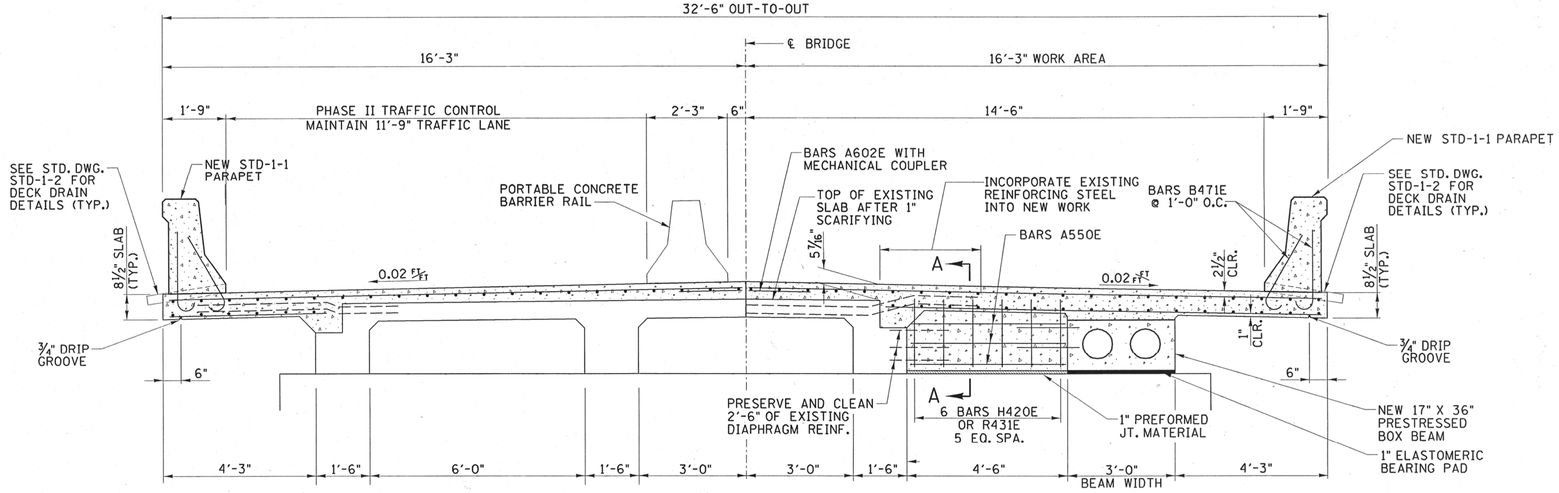



PHASE II DEMOLITION ✱

(REPLACE CANTILEVER ONE SIDE, REPLACE EXTERIOR BEAM AND CANTILEVER OTHER SIDE)
SCALE: ½" = 1'-0"




PHASE II CONSTRUCTION ✱

(REPLACE CANTILEVER ONE SIDE, REPLACE EXTERIOR BEAM AND CANTILEVER OTHER SIDE)
SCALE: ½" = 1'-0"

NOTE:
FOR SECTION A-A, SEE DWG. NO. BR-88-33.

✱ TYPICAL SPANS 2, 5, 8 AND 18

15.63

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

SR22 OVER NORTH REELFOOT CREEK
BRIDGE NO. 66-SR22-15.63

OBION COUNTY
2008

BR-88-27

GARVER ENGINEERS

| | | | |
|---|---|---|---|
| DESIGNED BY | S. F. HARPER | DATE | AUG. 2007 |
| DRAWN BY | J. P. DELK | DATE | AUG. 2007 |
| SUPERVISED BY | J. H. RUDDELL | DATE | AUG. 2007 |
| CHECKED BY | A. J. KHAIRI | DATE | AUG. 2007 |

TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

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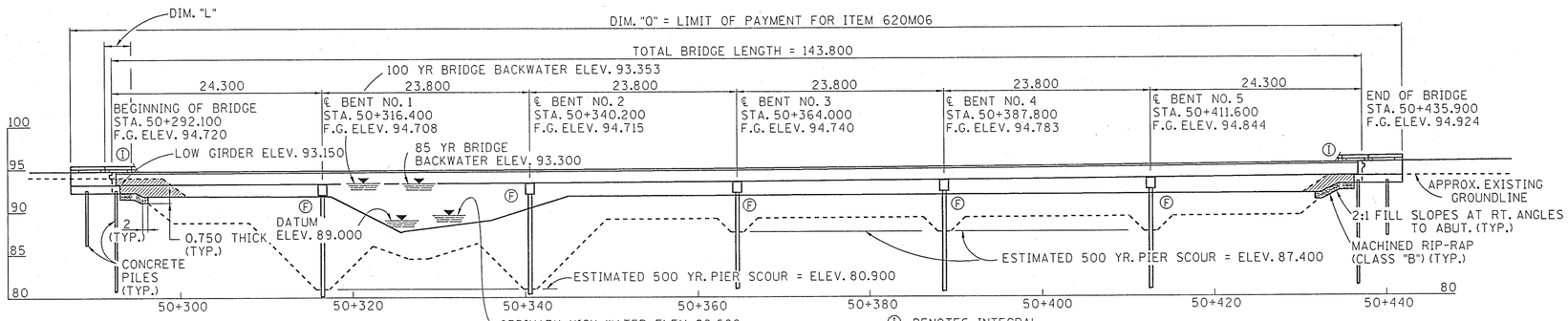
LAY126M.DGN

CONST. NO. 66012-3210-94

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| BR-STP-22(34) | 1999 | |

REVISIONS

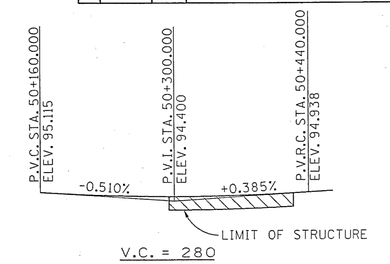
| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 5-21-99 | DJS | Rev. dates |



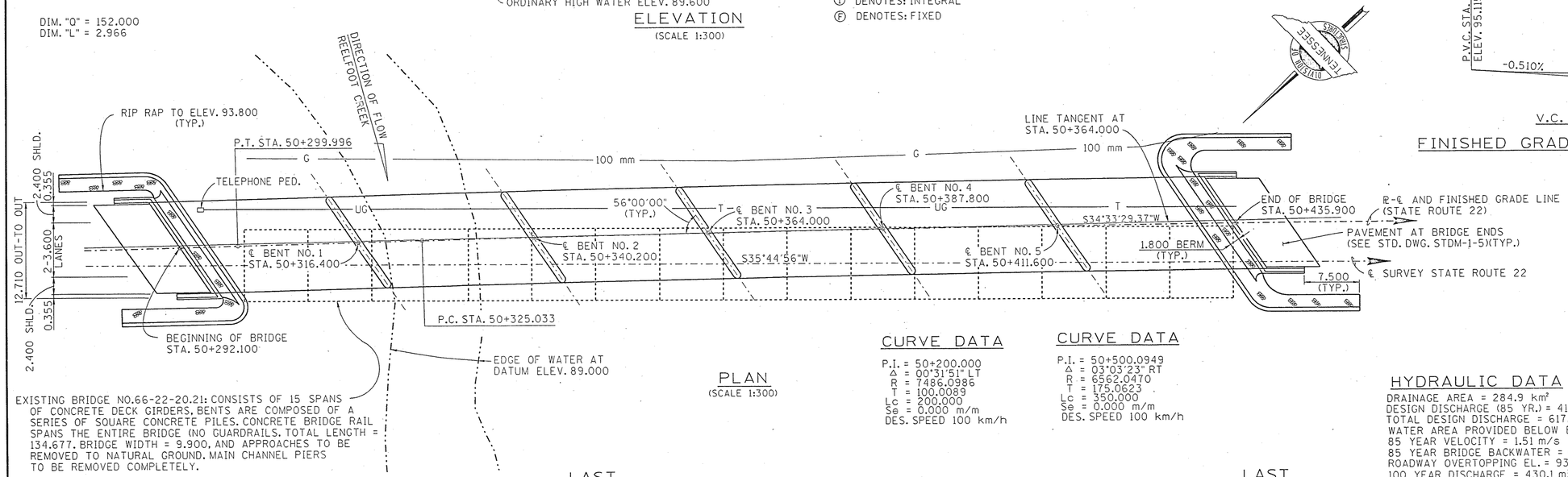

DIM. "O" = 152.000
DIM. "L" = 2.966

ELEVATION
(SCALE 1:300)

Ⓘ DENOTES: INTEGRAL
Ⓕ DENOTES: FIXED




FINISHED GRADE SKETCH S.R. 22




EXISTING BRIDGE NO.66-22-20.21: CONSISTS OF 15 SPANS OF CONCRETE DECK GIRDERS. BENTS ARE COMPOSED OF A SERIES OF SQUARE CONCRETE PILES. CONCRETE BRIDGE RAIL SPANS THE ENTIRE BRIDGE (NO GUARDRAILS. TOTAL LENGTH = 134.677. BRIDGE WIDTH = 9.900, AND APPROACHES TO BE REMOVED TO NATURAL GROUND. MAIN CHANNEL PIERS TO BE REMOVED COMPLETELY.

PLAN
(SCALE 1:300)

## CURVE DATA

P.I. = 50+200.000
Δ = 00°31'51" LT
R = 7486.0986
T = 100.0089
Lc = 200.000
Se = 0.000 m/m
DES. SPEED 100 km/h

## CURVE DATA

P.I. = 50+500.0949
Δ = 03°03'23" RT
R = 6562.0470
T = 175.0623
Lc = 350.000
Se = 0.000 m/m
DES. SPEED 100 km/h

## HYDRAULIC DATA

DRAINAGE AREA = 284.9 $km^2$
DESIGN DISCHARGE (85 YR.) = 416.8 $m^3/s$
TOTAL DESIGN DISCHARGE = 617.0 $m^3/s$
WATER AREA PROVIDED BELOW EL. 93.190 = 276.8 $m^2$
85 YEAR VELOCITY = 1.51 m/s
85 YEAR BRIDGE BACKWATER = 0.24 m @ EL. 93.300
ROADWAY OVERTOPPING EL. = 93.300 $m^3/s$
100 YEAR DISCHARGE = 430.1 $m^3/s$ @ EL. 93.350
TOTAL (100 YEAR) DISCHARGE = 637.0 $m^3/s$

## LIST OF DRAWINGS

| | DWG. NO. | LAST REV. DATE |
|---|---|---|
| LAYOUT | M-377-77 | 05-21-99 |
| ESTIMATED QUANTITIES | M-377-78 | 05-21-99 |
| GENERAL NOTES | M-377-79 | |
| FOUNDATION DATA | M-377-80 | |
| SUPERSTRUCTURE | M-377-81 | |
| SUPERSTRUCTURE DETAILS | M-377-82 | |
| SUPERSTRUCTURE DETAILS | M-377-83 | |
| PRESTRESSED I-BEAM DETAILS | M-377-84 | |
| ABUTMENT NO. 1 | M-377-85 | |
| ABUTMENT NO. 1 DETAILS | M-377-86 | |
| ABUTMENT NO. 2 | M-377-87 | |
| ABUTMENT NO. 2 DETAILS | M-377-88 | |
| BENTS NO. 1 THRU 5 | M-377-89 | 05-21-99 |
| FINAL FOUNDATION DATA | M-377-90 | |
| BILL OF STEEL | M-377-91 | 05-21-99 |

## LIST OF STANDARD DRAWINGS

| | DWG. NO. | LAST REV. DATE |
|---|---|---|
| PAVEMENT AT BRIDGE ENDS | STDM-1-5 | 04-28-97 |
| STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS | STDM-4-1 | 04-28-97 |
| STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA | STDM-4-2 | 06-10-96 |
| STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS | STDM-4-3 | 06-10-96 |
| STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS | STDM-4-4 | 06-10-96 |
| STANDARD PILE DETAILS | STDM-5-1 | 06-10-96 |
| STANDARD PILE DETAILS | STDM-5-2 | 06-10-96 |
| STANDARD SEISMIC DETAILS | STDM-6-1 | 06-10-96 |
| STANDARD CONCRETE BRIDGE RAIL | STDM-7-1 | 06-10-96 |
| REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB | STDM-9-1 | 06-10-96 |
| MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS | STDM-10-1 | 06-10-96 |
| STANDARD DETAILS FOR I-BEAMS | STDM-14-2 | 06-10-96 |

## LIST OF SPECIAL PROVISIONS

| | DWG. NO. | LAST REV. DATE |
|---|---|---|
| REGARDING APPROVAL OF SHOP DRAWINGS | 105A | 12-15-97 |

NOTE: ALL DIMENSIONS SHOWN IN METERS UNLESS OTHERWISE NOTED.

DIVISION OF PROJECT DEVELOPMENT METRIC

JOHN HOUSTON WALKER REGISTERED ENGINEER AGRICULTURE STATE OF TENNESSEE 3-15-99

2019 ADT = 2800
12m ROADWAY WITH STDM-7-1 BRIDGERAIL
DESIGN SPEED = 100 km/h

BRIDGE NO. 1
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT
STATE ROUTE 22
OVER
REELFOOT CREEK
BRIDGE I.D. NO. 66SR0220015
STATION 50+364.000 L.M. 20.21
OBION COUNTY
1999

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-377-77

DESIGNED BY DAN SHIKE DATE 03-98(02-98)
DRAWN BY F.F. FROST DATE 01-99
SUPERVISED BY J.H.W./R.L.H. DATE 01-99
CHECKED BY DAN SHIKE DATE 02-99

/usr/stevgr/steven/lay126m.dgn 13-MAR-1999 12:00

MACHINE RIP-RAP (CLASS B) = 364 TONNE

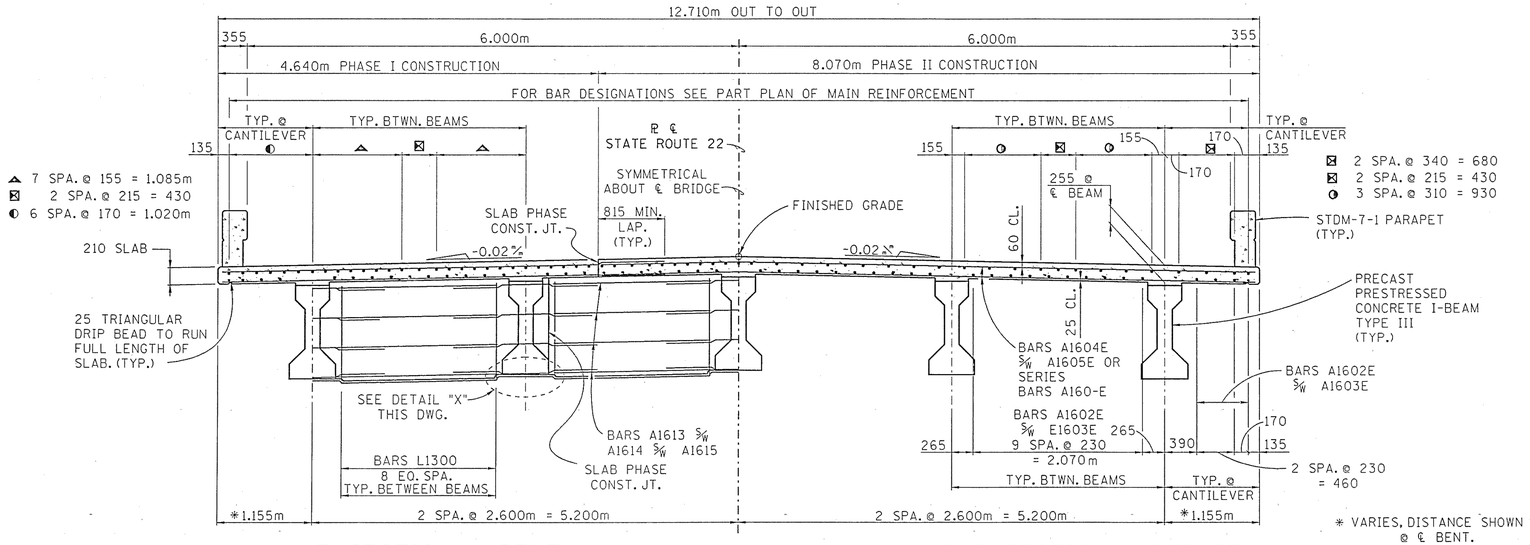
TEMPORARY MEDIAN BARRIER LENGTH = 158.000

CONST. NO. 66012-3210-94

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| BR-STP-22(34) | 1999 | |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |




HALF SECTION @ BENT

HALF SECTION @ MID-SPAN

## TYPICAL CROSS-SECTION

(LOOKING FORWARD ON SURVEY)

## SUPERSTRUCTURE GENERAL NOTES:

NOTE: NO PORTION OF THE BRIDGERAIL SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

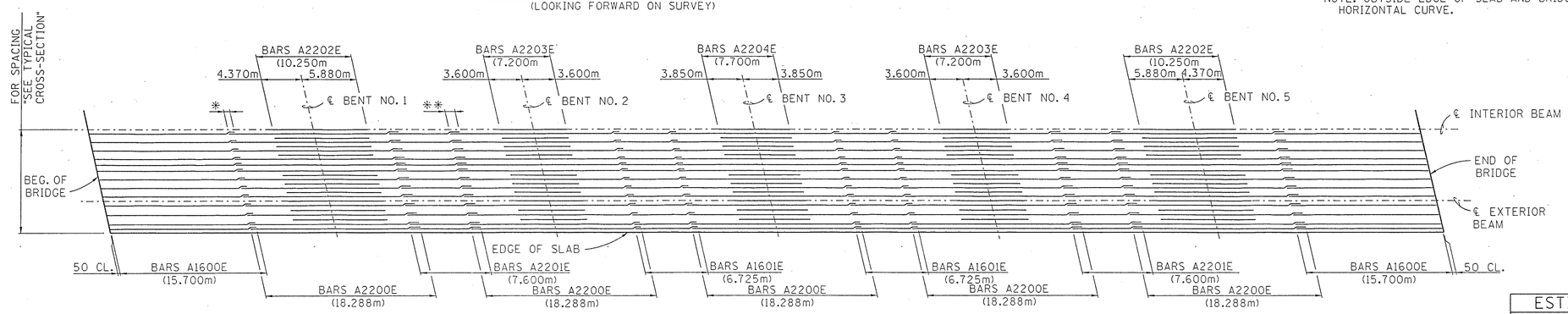
NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR BRIDGERAIL. THE BRIDGE RAIL SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO, SEE STANDARD DRAWING STDM-7-1.

NOTE: SUPPORT DIAPHRAGMS AT BENTS SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB AND INCLUDED IN THE QUANTITY FOR ITEM 604MO3.09.

SPECIAL NOTE FOR ANCHOR BOLTS AT BENTS: ANCHOR BOLT ASSEMBLIES AT BENTS SHALL BE IN ACCORDANCE WITH STANDARD DRAWING STDM-6-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

NOTE: OUTSIDE EDGE OF SLAB AND BRIDGE RAIL TO CONFORM TO HORIZONTAL CURVE.

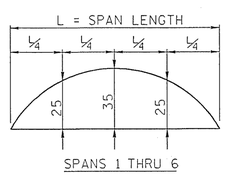



## PART PLAN OF MAIN REINFORCEMENT

* DENOTES: 600 MIN. LAP NO. 16 BAR

** DENOTES: 1.040m MIN. LAP NO. 22 BAR

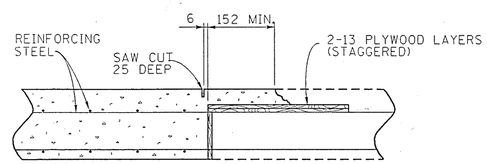
| ESTIMATED QUANTITIES | | |
|---|---|---|
| CLASS "D" CONCRETE (BRIDGE DECK) $m^3$ | EPOXY COATED REINFORCING STEEL kg | STEEL BAR REINFORCEMENT kg |
| 424 | 65863 | 1982 |




## DEAD LOAD CORRECTION CURVE

THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.

IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.



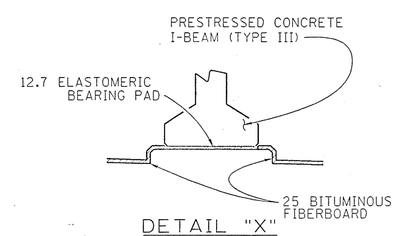

## SLAB CONSTRUCTION JOINT DETAIL

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 3.050m OR FURTHER THAN 4.572m FROM AN INTERIOR SUPPORT.

2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 4.572m OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.

ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN ABOVE.




DETAIL "X"

NOTE: ALL DIMENSIONS SHOWN IN MILLIMETERS, WITH THE EXCEPTION OF STATIONS AND ELEVATIONS, UNLESS OTHERWISE NOTED.

BRIDGE NO. 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE
STATE ROUTE 22
OVER
REELFOOT CREEK
STATION 50+364.000
OBION COUNTY
1999

DESIGNED BY DAN SHIKE DATE 03-98
DRAWN BY J E DODSON DATE 11-98
SUPERVISED BY A.E.P./R.L.H. DATE 11-98
CHECKED BY DAN SHIKE DATE 02-99

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-377-81

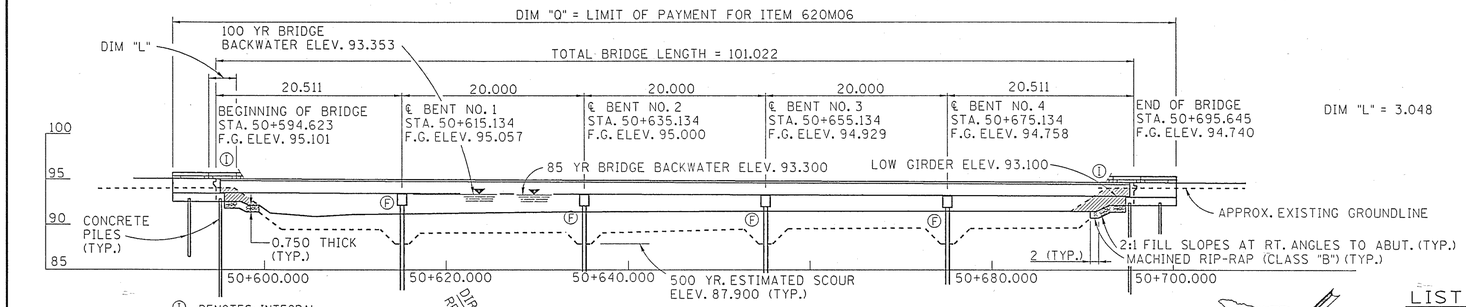
c:\users\johnh\tsup\sup126m.dgn
(MAR-1999)
1031

LAY127M.DGN

| CONST. NO. 66012-3210-94 | | |
|---|---|---|
| PROJECT NO. | YEAR | SHEET NO. |
| BR-STP-22(34) | 1999 | |

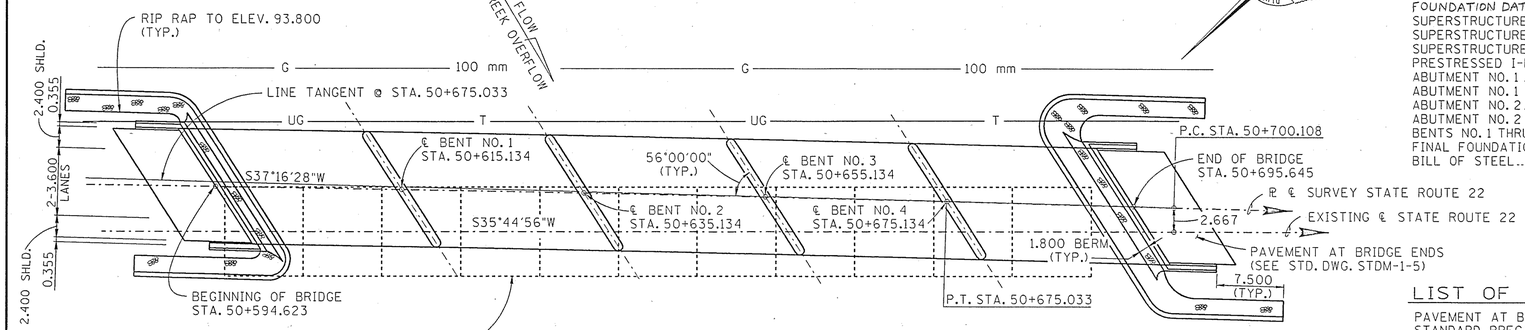
REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| 1 | 05-21-99 | DJS | Add Foundation Data |




ELEVATION
(SCALE 1:300)

▨ DENOTES: AREA TO BE EXCAVATED AND PAID FOR UNDER ROADWAY ITEM NO. 203M01 (TYP.)




EXISTING BRIDGE NO.66-22-20.39: CONSISTS OF 11 SPANS OF CONCRETE DECK GIRDERS. BENTS ARE COMPOSED OF A SERIES OF SQUARE CONCRETE PILES. CONCRETE BRIDGE RAIL SPANS THE ENTIRE BRIDGE (NO GUARDRAILS. TOTAL LENGTH = 95.291. BRIDGE WIDTH = 9.900, AND APPROACHES TO BE REMOVED TO NATURAL GROUND.

PLAN
(SCALE 1:300)

## LIST OF DRAWINGS

| | DWG. NO. | LAST REV. DATE |
|---|---|---|
| LAYOUT | M-377-92 | 05-21-99 |
| GENERAL NOTES | M-377-93 | |
| ESTIMATED QUANTITIES | M-377-94 | 05-21-99 |
| FOUNDATION DATA | M-377-95 | 05-21-99 |
| SUPERSTRUCTURE | M-377-96 | |
| SUPERSTRUCTURE DETAILS | M-377-97 | |
| SUPERSTRUCTURE DETAILS | M-377-98 | |
| PRESTRESSED I-BEAM | M-377-99 | |
| ABUTMENT NO. 1 | M-377-100 | |
| ABUTMENT NO. 1 DETAILS | M-377-101 | |
| ABUTMENT NO. 2 | M-377-102 | |
| ABUTMENT NO. 2 DETAILS | M-377-103 | |
| BENTS NO. 1 THRU 4 | M-377-104 | |
| FINAL FOUNDATION DATA | M-377-105 | |
| BILL OF STEEL | M-377-106 | |

## LIST OF STANDARD DRAWINGS

| | DWG. NO. | LAST REV. DATE |
|---|---|---|
| PAVEMENT AT BRIDGE ENDS | STDM-1-5 | 04-28-97 |
| STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS | STDM-4-1 | 04-28-97 |
| STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA | STDM-4-2 | 06-10-96 |
| STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS | STDM-4-3 | 06-10-96 |
| STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS | STDM-4-4 | 06-10-96 |
| STANDARD PILE DETAILS | STDM-5-1 | 06-10-96 |
| STANDARD PILE DETAILS | STDM-5-2 | 06-10-96 |
| STANDARD SEISMIC DETAILS | STDM-6-1 | 06-10-96 |
| STANDARD CONCRETE BRIDGE RAIL | STDM-7-1 | 06-10-96 |
| REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB | STDM-9-1 | 06-10-96 |
| MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS | STDM-10-1 | 06-10-96 |
| STANDARD DETAILS FOR I-BEAMS | STDM-14-2 | 06-10-96 |

## LIST OF SPECIAL PROVISIONS

| | DWG. NO. | LAST REV. DATE |
|---|---|---|
| REGARDING APPROVAL OF SHOP DRAWINGS | 105A | 03-06-95 |

## HYDRAULIC DATA

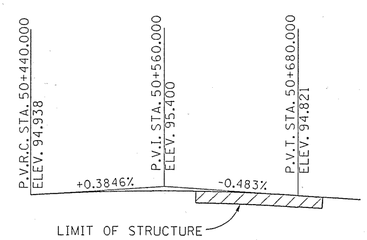
DRAINAGE AREA = 284.9 km
DESIGN DISCHARGE (85 YR.) = 182.9 m³/s
TOTAL DESIGN DISCHARGE = 617.0 m³/s
WATER AREA PROVIDED BELOW EL. 93.190 = 207.0 m²
85 YEAR VELOCITY = 0.88 m/s
85 YEAR BRIDGE BACKWATER = 0.24 m @ EL. 93.300
ROADWAY OVERTOPPING EL. = 93.300
100 YEAR DISCHARGE = 181.9 m³/s @ EL. 93.350
TOTAL (100 YEAR) DISCHARGE = 637.0 m³/s

## CURVE DATA

P.I. = 50+500.0949
Δ = 03°03'23" RT
R = 6562.0470
T = 175.0623
Lc = 350.000
Se = N.C.
DES. SPEED 100 km/h

## CURVE DATA

P.I. = 50+800.1186
Δ = 01°31'39" LT
R = 7501.9996
T = 100.0089
Lc = 200.000
Se = N.C.
DES. SPEED 100 km/h




V.C. = 240

FINISHED GRADE SKETCH S.R. 22

DIVISION OF METRIC APPLICATION

NOTE: ALL DIMENSIONS SHOWN IN METERS UNLESS OTHERWISE NOTED.
2019 ADT = 2800
12 m ROADWAY WITH STDM-7-1 (OPEN) BRIDGERAIL
DESIGN SPEED = 100 km/h

BRIDGE NO. 2
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT
STATE ROUTE 22
OVER
REELFOOT CREEK OVERFLOW
BRIDGE I.D. NO. 66SR0220017
STATION 50+645.134 L.M. 20.39
OBION COUNTY
1999

JOHN HOUSTON WALKER
STATE OF TENNESSEE
3-15-99

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

| | | | |
|---|---|---|---|
| DESIGNED BY | K. MARTIN | DATE | 4-98(02-98) |
| DRAWN BY | F.F. FROST | DATE | 01-99 |
| SUPERVISED BY | J.H.W./R.L.H. | DATE | 01-99 |
| CHECKED BY | K. MARTIN | DATE | 02-99 |

M-377-92

/usr/3steven/steven/lay127m.dgn
15-MAR-1999
13:29

MACHINE RIP-RAP (CLASS B) = 388 TONNE

TEMPORARY MEDIAM BARRIER LENGTH = 115.652m

SUP127M.DGN

CONST. NO. 66012-3210-94

| PROJECT NO. | YEAR | SHEET NO. |
|---|---|---|
| BR-STP-2(34) | 1999 | |

REVISIONS

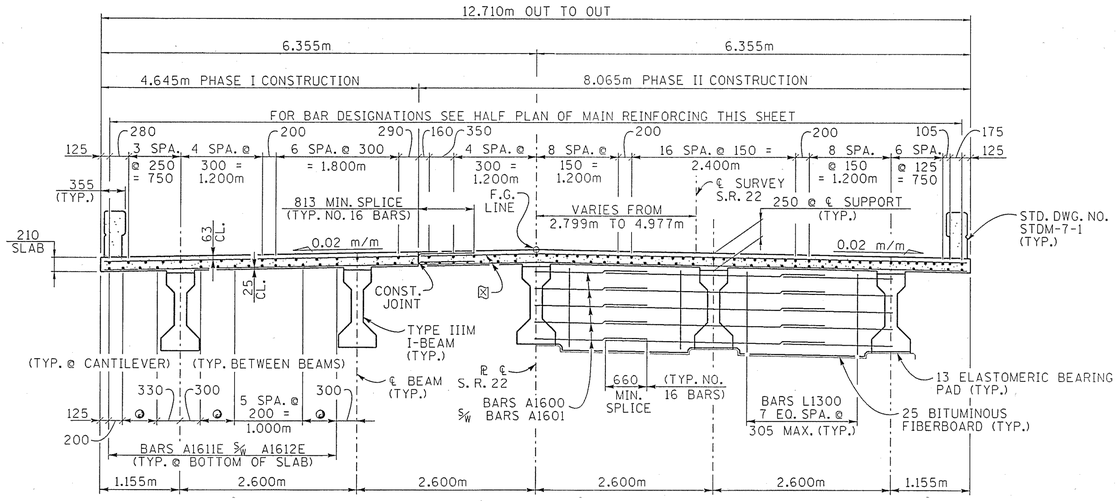
| NO. | DATE | BY | BRIEF DESCRIPTION |
|---|---|---|---|
| | | | |

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: OUTSIDE EDGE OF SLAB AND BRIDGE RAIL TO CONFORM TO HORIZONTAL CURVE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR BRIDGE RAIL. THE BRIDGE RAIL SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO SEE DRAWING NO. STDM-7-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED PRIOR TO PLACING ANY LOADS ON THE GIRDERS. HOWEVER, TEMPORARY ERECTION DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.



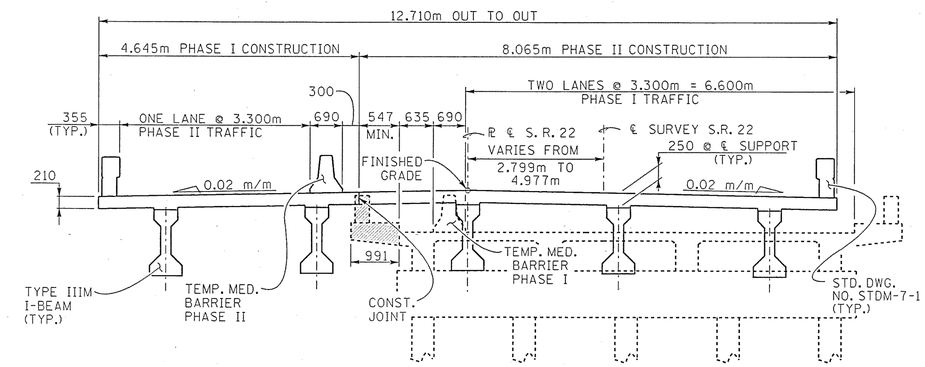

◑ DENOTES 2 SPA. @ 250 = 500.

⊠ SEE "SLAB PLAN" ON DWG. NO. M-377-98 FOR BAR DESIGNATIONS AND DETAILS.

NOTE: OFFSET ALL REINFORCEMENT TO ACCOMMODATE A 50 MINIMUM CLEARANCE FROM THE PHASE CONSTRUCTION JOINT.

## TYPICAL CROSS SECTION

(LOOKING FORWARD ON SURVEY)

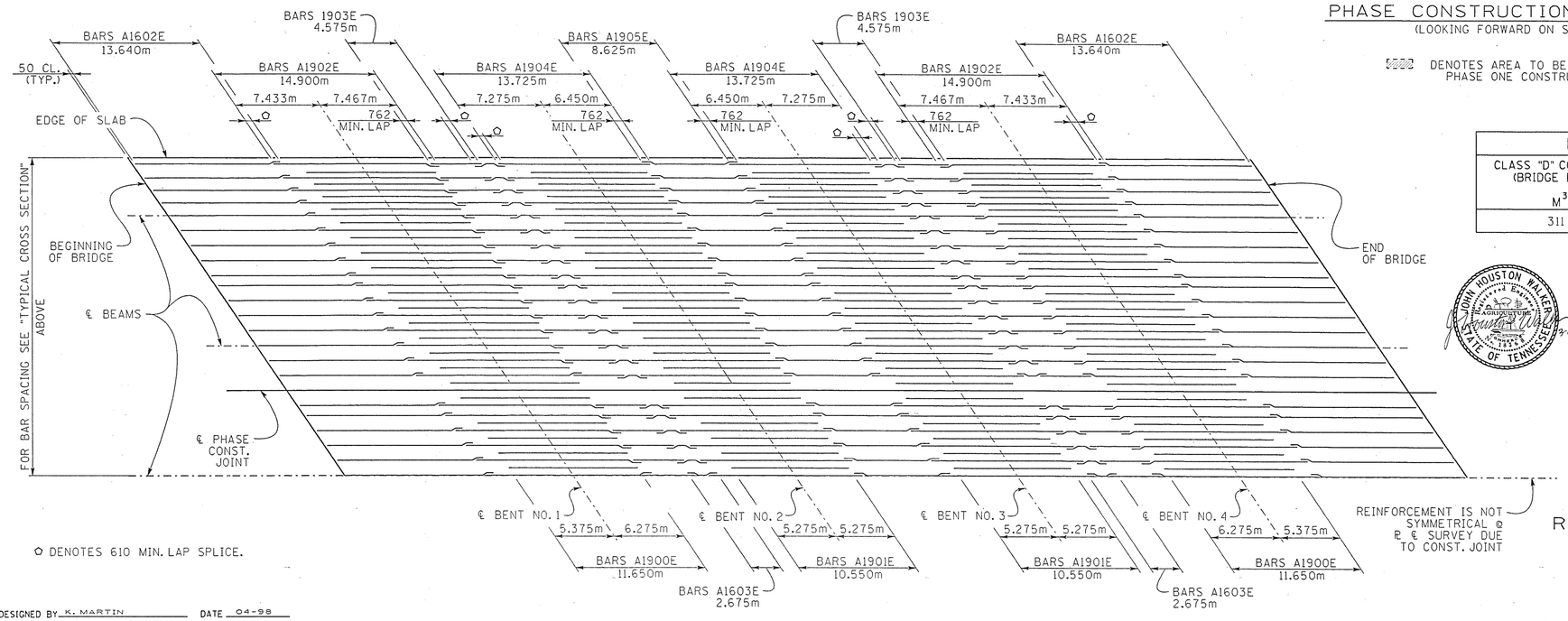



## PHASE CONSTRUCTION SEQUENCE

(LOOKING FORWARD ON SURVEY)

▨ DENOTES AREA TO BE REMOVED PRIOR TO PHASE ONE CONSTRUCTION.

| ESTIMATED QUANTITIES | | |
|---|---|---|
| CLASS "D" CONCRETE (BRIDGE DECK) $M^3$ | STEEL BAR REINFORCEMENT (BRIDGES) KGS. | EPOXY COATED REINFORCING STEEL KGS. |
| 311 | 1,471 | 49,063 |




✿ DENOTES 610 MIN. LAP SPLICE.

## HALF PLAN OF MAIN REINFORCEMENT

JOHN HOUSTON WALKER STATE OF TENNESSEE 9-15-99

DIVISION OF STRUCTURES METRIC

NOTE: ALL DIMENSIONS SHOWN IN MILLIMETERS, WITH THE EXCEPTION OF STATIONS AND ELEVATIONS, UNLESS OTHERWISE NOTED.

BRIDGE NO. 2

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE
STATE ROUTE 22
OVER
REELFOOT CREEK OVERFLOW
STATION 50+645.134
LOG MILE 20.39
OBION COUNTY
1999

| | | | |
|---|---|---|---|
| DESIGNED BY | K. MARTIN | DATE | 04-98 |
| DRAWN BY | J. O'REE | DATE | 12-98 |
| SUPERVISED BY | R.L.H. / A.E.P. | DATE | 12-98 |
| CHECKED BY | K. MARTIN | DATE | 02-99 |

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-377-96

/usr/stygn/steven/sup127m.dgn
09-14-1999
09:34